

# CITY OF WESTFIELD DOWNTOWN REDEVELOPMENT PLAN

FALL 2025





*As Designers of Possibility, Browning Day continually explores the use of new and emergent technologies in our professional work. Browning Day uses Generative AI technology in some portions of our work. Some images within this document were created in whole or in part with Generative AI tools. Images which feature the glyph pictured here were created with this technology. These images are subject to the same general copyright and rights restrictions as the rest of the document. They are not subject to any additional copyright restrictions; use them with attribution as you would with any other image, graphic, or text within the document.*

# CITY OF WESTFIELD DOWNTOWN REDEVELOPMENT PLAN

---

OCTOBER 7, 2025

Produced For Exclusive Use by the City of Westfield

© 2025 Browning Day All Rights Reserved  
626 North Illinois Street, Indianapolis, Indiana 46204 | (317) 635-5030 | [www.browningday.com](http://www.browningday.com)

## INTRODUCTION

Executive Summary	v
Document Structure	vi

## PART ONE: MASTER PLAN

The Vision	1
Connectivity	5
Plan Areas	11
Focused Vision Areas	
Grassy Branch Creekway	15
Main Street, Westfield, Indiana	21
Bank Block	25
Downtown Carnegie Plaza	29
Downtown History Walk	33
Infill Housing	35
Welcome to Westfield	37

## PART TWO: IMPLEMENTATION & MAINTENANCE

Development Implementation and Phasing	39
Catalyst Projects	45
Carnegie Plaza Block	46
Garage Integration	47
Key Projects	48
Horizon Projects	51
Redevelopment Plan Maintenance	53

## PART THREE: STANDARDS & GUIDELINES

Applicability	55
Definitions & How to Use This Section	56
Plan Areas	57
Permitted Uses	58
General Development Standards & Guidelines	
Architecture	61
Circulation	79
Site & Landscape	93
Plan-Area Development Guidelines and Standards	
Downtown Core	103
Main Street	105
Legacy and Historic Core	109
Residential Infill Areas	111
Residential Creekside	113
Grand Junction District	115
Downtown Adjacent	117
Westfield Boulevard	119
Grassy Branch Creekway	121

## APPENDICIES

Acknowledgements	A-1
Development Data	B-1
Inventory & Analysis	C-1

## TABLE OF CONTENTS



City of Westfield



**Browning Day**  
Master Planner



**Skender**  
Construction Consultant



**Company Name**  
Engineering Consultant

## PROJECT TEAM

---



# INTRO

**A VISION FOR DOWNTOWN**

WELCOME TO WESTFIELD

# INTRODUCTION | WELCOME TO WESTFIELD

---

## WHAT

This Redevelopment Plan is a vision for the growth of downtown Westfield over the course of the next two plus decades. The Plan includes a comprehensive, long-term blueprint that outlines the goals, strategies, and actions to execute this vision.

## WHY

A redevelopment plan is a roadmap for the future, guiding development and growth in a manner consistent with the needs, wants, and values of the community. It ensures that growth and change are planned, cohesive, and aligned with the values of the people who live and work there. By having a clear vision, cities can make wise decisions about investments, attract funding, and guide development in a direction that benefits the community as a whole.

## HOW

By gathering data and analysis of existing conditions, planners and designers can set specific measurable goals that align with the desires of the community and the broader vision for the future. The final plan is adopted and guides development and policy-making.

## NOW WHAT?

To make this vision a reality, two crucial steps need to be taken: 1) working with developers, businesses, and community groups to bring the vision to life, and 2) monitor progress and adjust the plan accordingly based on results.

## EXECUTIVE SUMMARY

The Downtown Redevelopment Plan showcases a vision for a transformation of Westfield's Downtown into a vibrant, walkable community that harmoniously blends modern living with memorable charm. Westfield ranked as America's 6<sup>th</sup> fastest growing Large City in a 2022 Census Report, and development and growth have not stopped since, but that development has been scattershot and without a clear focus, especially in the downtown core. This Redevelopment Plan will re-focus that energy and effort, guiding Westfield into the future. Central to this vision is the revitalization of the existing Downtown area and the expansion of development eastward along SR 32. Pedestrian access is the foremost priority, with emphasis on walkability - particularly along Grassy Branch Creek - which will be transformed into a central feature with inviting pathways and green spaces. The result is a place that is uniquely Westfield, Indiana.

### *Downtown will be...* **A Walkable Community that Connects Memorable Places**

Encourage visual connectivity, intrigue, and activation especially along missing pieces of the urban fabric. Provide a mix of uses supported by sidewalks, crosswalks, multi-modal routes, and paths that create a safe place for pedestrians. Provide convenient, well-located parking that encourages people to park and walk to their destination.

### *Downtown will...* **Capitalize on the Green Investments and Amenities**

Tie together existing park amenities with new natural systems and places; water, tree canopy, riparian zones. All stitched together within active and passive parks.

### *Downtown will...* **Shift the Center of Development to Create Identity, Places, and Experiences**

Craft a public realm that evokes positive emotion and "wow factor." High-quality architecture should be blended with the traditional charm of Midwest hometowns. History must be acknowledged. Allow space for unique threshold moments that instantly let you know you are Downtown. Zone and district the area in a way that encourages development in the "center" of Downtown. Ensure those developments are appropriate for Downtown and varied for a diverse set of users.



## PLAN AREA MAP

This map shows the extents of the downtown area and the Plan Areas defined and expanded in this document.

## DOCUMENT STRUCTURE

### PART ONE: THE VISION

Part one of the Redevelopment Plan showcases the vision for the future of Downtown Westfield. The Vision highlights the key overarching framework of the Downtown Plan, Connectivity and Identity within the Plan Areas, and then zooms in on the Focused Visions Areas of Downtown. Focused Vision Areas are the places, spaces, and concepts that have unique and key significance to the future of the downtown.

### PART TWO: IMPLEMENTATION AND MAINTENANCE

This section outlines the paths to implementation for the various key areas, developments, and projects within Downtown Westfield. Catalyst, Key, and Horizon projects are identified and quantified. Also in this section is a guide for the long-term maintenance of the plan.

### PART THREE: STANDARDS & GUIDELINES

Part three is a comprehensive development guide for all future development within all plan areas of Downtown Westfield. General, Architectural, and Site specific standards and guidelines are identified and illustrated.

### APPENDICES

- Appendix A: Acknowledgments
- Appendix B: Development Data
- Appendix C: Discovery and Analysis Summary





# ONE

**A VISION FOR DOWNTOWN**  
MASTER PLAN

## A VISION FOR DOWNTOWN | MASTER PLAN



### UNIQUELY WESTFIELD

The Downtown Redevelopment Plan showcases a vision for a transformation of Westfield's Downtown into a vibrant, walkable community that harmoniously blends modern living with memorable charm. Central to this vision is the revitalization of the existing Downtown area and the expansion of development eastward along SR 32. Pedestrian access is the foremost priority, with emphasis on walkability - particularly along Grassy Branch Creek - which will be transformed into a central feature with inviting pathways and green spaces. The result is a place that is uniquely Westfield, Indiana.



## CHALLENGES TO OPPORTUNITIES

In the early stages of the master planning process, a number of challenges were identified that play a critical role in shaping the success of the Downtown. These criteria (below) eventually turned into the core opportunities of the Redevelopment Plan.

### A Walkable Community

Provide a mix of uses supported by sidewalks, crosswalks, multi-modal routes, and paths that create a safe place for pedestrians. Provide convenient, well-located parking that encourages people to park and walk to their destination.

### Attachment to Place

Craft a public realm that evokes positive emotion and “wow factor.” High-quality architecture should be blended with the traditional charm of Midwest hometowns. History must be acknowledged.

### Unique Downtown Identity

Allow space for unique threshold moments that instantly let you know you are Downtown.

### Capitalize on the Green Investment

Tie together existing park assets with new natural systems and places; water, tree canopy, riparian zones. All stitched together within active and passive parks.

### Connecting “Here” to “There”

Encourage visual connectivity, intrigue, and activation especially along missing pieces of the urban fabric.

### Shift the Center of Development

Zone and district the area in a way that encourages development in the “center” of Downtown. Ensure those developments are appropriate for Downtown and varied for a diverse set of users.

## THE VISION

Historic preservation is a cornerstone of this Redevelopment Plan. Iconic structures such as the Carnegie Library and the Bank Building are preserved, maintaining the city’s rich heritage. Grassy Branch Creek is activated with a new trail system and publicly accessible features, turning it into a vibrant and engaging focal point. Parking solutions are thoughtfully integrated, with public garages along Penn Street and Jersey Street, where they best support pedestrian movement and density. Structured parking is prioritized to address the needs of residents and businesses.

The redevelopment of Downtown introduces urban density with buildings rising up to 5-6 stories, incorporating a mix of residential and commercial spaces. City blocks are reorganized for larger-scale projects, creating a dynamic environment for living and business.

Owner-occupied living is provided in high-quality varied residential architecture, including row homes that frame parks and linear spaces. To balance density and TIF funding opportunities, 4-story single-family residences or stacked flats are encouraged, with options for premium features such as rooftop decks and private patios as market conditions allow.

Architectural excellence is a hallmark of this redevelopment, with a variety of styles to ensure a unique and culturally resonant Downtown. Street design incorporates trails and multi-modal paths, with parallel on-street parking where possible. Ultimately, this Redevelopment Plan aims to create a lively, engaging Downtown that serves as a destination for residents and visitors alike, enhancing the quality of life and fostering a strong sense of community.

## DOWNTOWN FOCUSED VISION AREAS | LEGEND

- |          |                               |          |  |
|----------|-------------------------------|----------|--|
| <b>A</b> | Grassy Branch Creekway        | <b>G</b> | East Street Residential Neighborhood   |
| <b>B</b> | Main Street Plan Area - North | <b>H</b> | Creekside Plan Areas                   |
| <b>C</b> | Main Street Plan Area - South | <b>I</b> | Grand Junction Plan Area               |
| <b>D</b> | Bank Block Development        | <b>J</b> | Historic and Legacy Plan Area          |
| <b>E</b> | Carnegie Plaza Development    | <b>K</b> | Westlea Residential Neighborhood       |
| <b>F</b> | West Penn Blocks              | <b>L</b> | Cherry Street Residential Neighborhood |



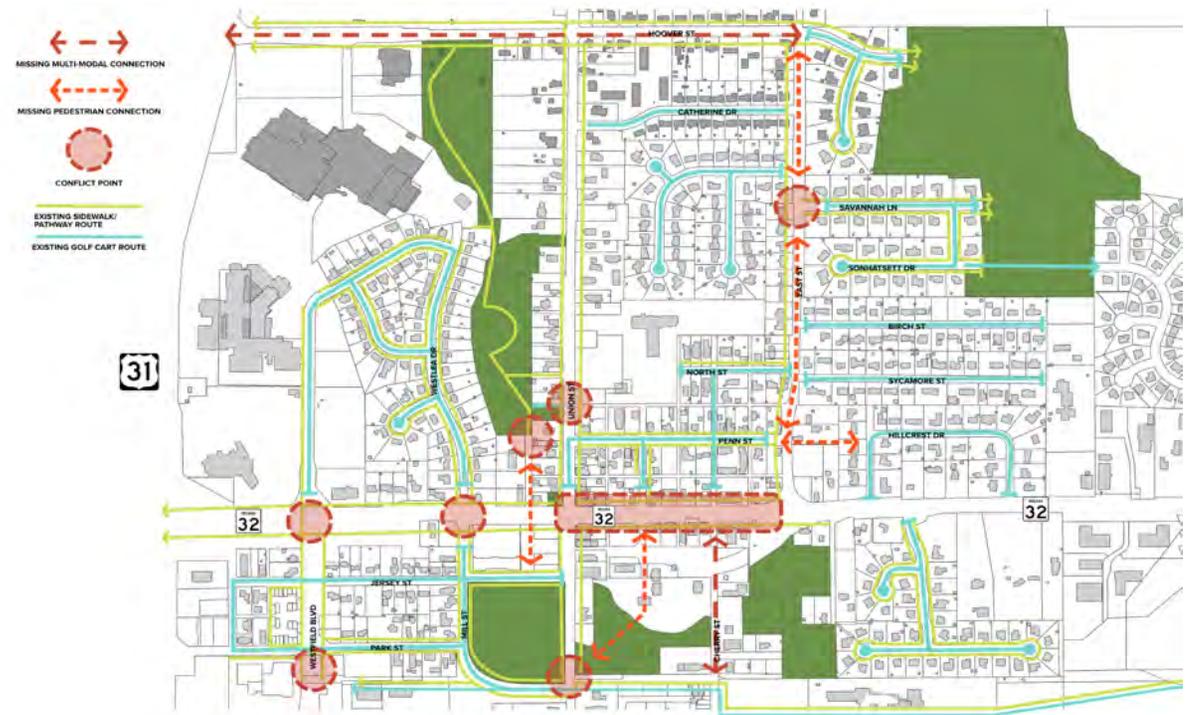
Master Plan Aerial View

## CONNECTIVITY | LINKING DOWNTOWN TO WESTFIELD

### STITCHING DOWNTOWN INTO THE REST OF WESTFIELD

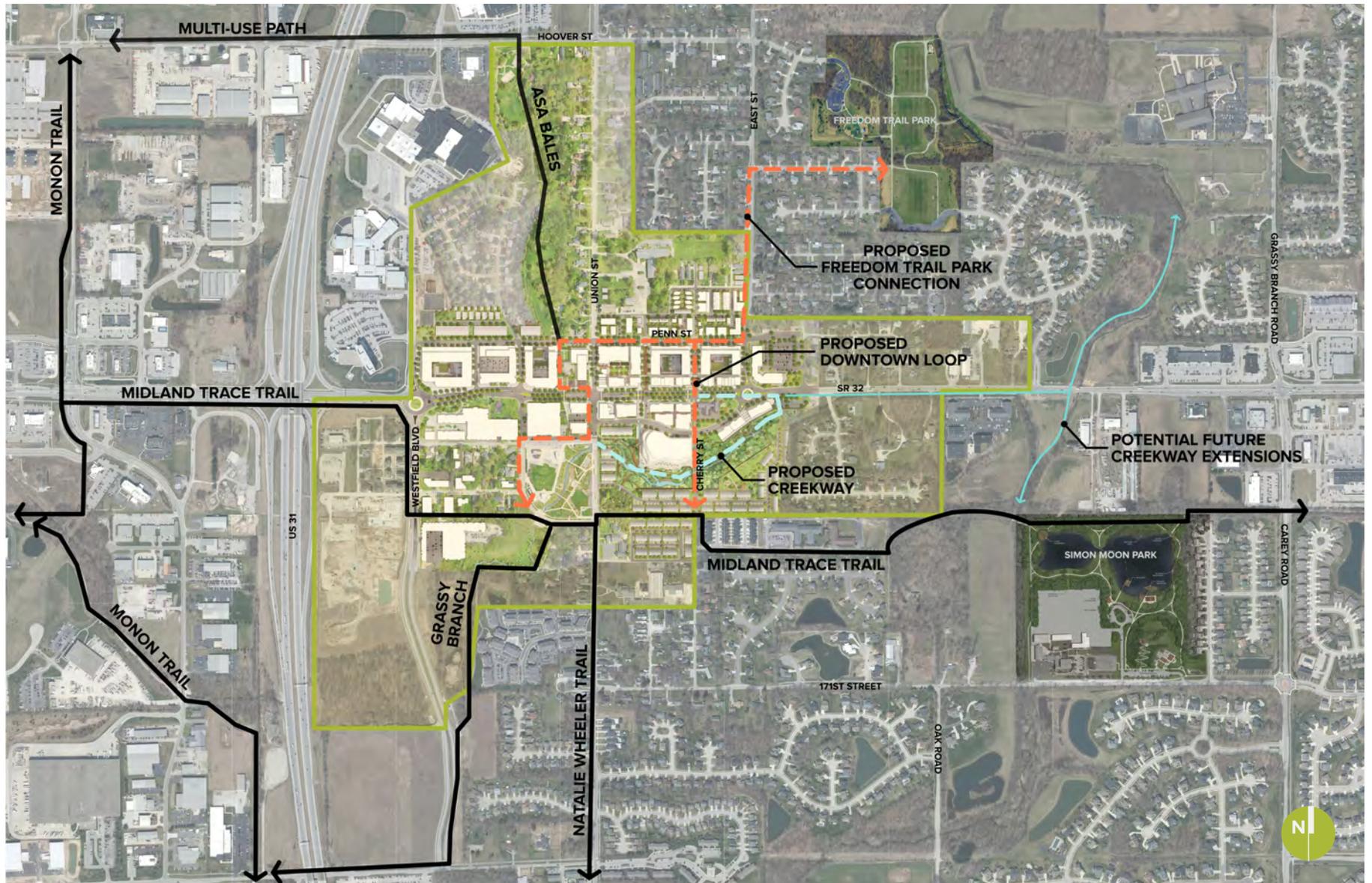
Analyzing the existing fabric of Downtown early in the planning process led to a quick conclusion about the nature of connectivity from Downtown to the rest of the City; there was a key gap. Existing robust trails either bypassed Downtown completely (Monon) or they arrived on the periphery and then ended, dropping users off near Downtown but without a clear path forward (Midland Trace, Asa Bales). This gap is addressed through a series of trails, multi-use pathways, public plazas, and extensions of the City grid. The result is a network of connectivity that links the new public spaces, existing City developments and parks, and new assets to create a cohesive and linked Downtown.

Key within that network is the Downtown Loop. The Loop is a linked path of multi-use trails that acts as a pedestrian and bike “ring road” around Downtown. In the southeast, the Downtown Loop picks up arrivals from the Midland Trace Trail and moves them north, up Cherry Street. The new creekway joins up nearby, immediately allowing users a mix of options. From there the path crosses SR 32 at a predetermined INDOT crossing and heads north to Penn Street. Moving west along Penn Street, the Loop passes retail, office, townhouse, and apartment uses and eventually brings users to the Carnegie Plaza and a new public plaza. Here the Loop connects to the Asa Bales trail, pedestrians from western Penn, and the Legacy neighborhoods to the north.



*Early on in the planning process, gaps in the connectivity fabric of the Downtown core were identified. These gaps are in sidewalk and pedestrian ways, golf cart and multi-modal trails, and in some cases, conflicts with pedestrians and vehicles. The Master Plan begins to stitch those gaps together with a new connectivity thread.*

The Downtown Loop then moves south through the Carnegie Plaza, connecting to the historic Carnegie Library. Using the primary at-grade crossing at Union and SR 32, the Trail crosses that major east-west axis and connects south to the new restaurant plaza and Grand Junction Plaza. From there users reconnect with the Midland Trace Trail and Grassy Branch Trail, accessing other connections and parks to the south and west.



Downtown Connectivity Map



Fig 1.01 - Clear and identifiable signage makes golf cart traffic navigation easy through the Downtown and beyond.

## EXPANDED CONNECTIVITY

The expanded use of golf carts in Westfield has provided another mode of transportation, potentially creating the opportunity for thousands of people to visit Downtown without relying on a personal vehicle. The carts are permitted on roads with a speed limit of less than 25 mph and on the Midland Trace Trail. Enhancements that promote golf cart usage as an additional mode of transportation include pavement markings indicating their shared use on the roadway, general wayfinding, and designated cart parking both on and off street. On-street parking provides three golf cart spaces where a personal vehicle could typically be parked. Additionally, the cart parking locations at the corners provide better visibility for drivers entering and exiting side streets, as carts are not visually obstructive like a typical vehicle. As part of the connectivity network, an extension from the Downtown Loop Trail to Freedom Trail Park provides an off-street space for carts to travel, because East Street speed limits are too high for cart usage.



Fig 1.02 - Multi-modal paths that accommodate pedestrians, bikes, and other non-motorized vehicles serve as the foundation of the connectivity plan.



Fig 1.03 - Enhanced and comfortable pedestrian sidewalk and amenity spaces in Downtown bring activity to the streets.



Fig 1.04 - A variety of walkways and experiential paths are stitched throughout Downtown including boardwalks within the Grassy Branch Creekway.



Fig 1.05 - Pathways lead to larger and active public plazas, parks, and amenity spaces. Site design can act as a means of wayfinding, with trees, landscape features, and structures all oriented to provide a visual pathway through urban open spaces.

## PENN STREET | STREET + TRAIL SECTION



### DOWNTOWN LOOP - PENN STREET

Penn Street is envisioned to have a typical street section of two 11' travel lanes with parallel parking on each side of the street. The north side of Penn Street will have a typical 5' residential sidewalk, while the south side hosts the shared-use path of the Downtown Loop Trail. Bumpouts at pedestrian crossings are preferred. Additional planting areas or cafe seating may also occur within the typical parking lane depending on the context. Lane markings and signage will indicate that golf carts are permitted to be driven on Penn Street, and golf cart parking may occur in place of some vehicle parking spaces.

### DOWNTOWN LOOP STREETSCAPES

The establishment of a Downtown Loop Trail creates a vital link that unites several existing community trails into a cohesive network. By completing this connection, the Loop ensures that residents and visitors can easily navigate between parks, neighborhoods, and the Downtown core without interruption. Designed with consistent paving materials and wayfinding, the Trail provides a recognizable and continuous route through the heart of Downtown.

The Trail is intended to be a minimum of 12' wide and be used by pedestrian, cyclists, and other micro-mobility users, like scooters. Golf carts are not permitted on the trail, as they are intended to use the street for circulation within Downtown.

The Trail is to have regularly occurring amenities such as seating opportunities, waste bins, bike racks, and pet waste receptacles. Other features, depending on the context, include cafe seating, art and sculptures, additional and/or unique seating, and informal play opportunities.

As Downtown redevelopment occurs the trail will be a vital part of the growth and will form a solid foundation for new public spaces to build from.

## CHERRY STREET | STREET + TRAIL SECTIONS

### DOWNTOWN LOOP - CHERRY STREET

With the new SR 32 design introducing medians through the Downtown, Cherry Street is limited to right-in, right-out access onto SR 32. This provides an opportunity to provide unique design solutions for the north and south ends of Cherry Street. While both maintain a minimum 12' wide shared-use path as part of the Downtown Loop Trail, their configuration is primarily determined by adjacent uses and existing right-of-ways.

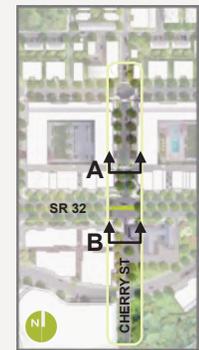
#### Cherry Street North

In addition to a shared-use path, Cherry Street north has a typical street section of two 11' travel lanes and a lane of parallel parking on each side of the street. Bumpouts at pedestrian crossings promote safety for people crossing the street. Cherry Street has right-in right-out access to and from SR 32.

#### Cherry Street South

The vision for Cherry Street south is to have a right-in only configuration onto a one-way south Cherry Street, and street parking on the western side of the street. This configuration runs a short distance until the intersection with Jersey Street. Walnut Street to the west is one-way north from Jersey Street to create a balanced loop to and from SR 32.

Cherry Street south of SR 32 is challenged by existing development at the edge of its eastern right-of-way. Because of this limitation, any expansion of the right-of-way will likely need to occur to the west.



## EAST STREET | STREET + TRAIL SECTION



### FREEDOM TRAIL PARK CONNECTION - EAST STREET

While the travel lanes on East Street are anticipated to remain as they are today, the sidewalk on the west side of the street is to be expanded to provide a shared-use path. This path is intended to be shared among pedestrians, cyclists, golf carts, and other non-vehicular modes of transportation. As such, the minimum width of this path is to be at least 14'. Signage will indicate that pedestrians should have priority and that cyclists and golf carts are to yield to them. Wayfinding signage can indicate distance and direction to and from Freedom Trail Park. Additional right-of-way or easements will need to be acquired from property owners for the path to be constructed.

### FREEDOM TRAIL PARK CONNECTION

As part of the Redevelopment Plan, a shared-use path is envisioned to be established on the western side of East Street to enhance and strengthen the connection between Downtown and Freedom Trail Park. Enhancing the connection between these two community assets creates a safe, direct, and accessible route between a vibrant community node and a valued green space.

The benefits of this connection extend beyond mobility. By linking these two assets, the network of safe and accessible bicycle/pedestrian networks is extended. This easier access promotes additional use, which in turn promotes a healthier lifestyle.

Ultimately, the connection ensures that Downtown and the park function not as separate destinations but as integrated parts of community life.

## PLAN AREAS | PLACEMAKING IN DOWNTOWN

### A COHESIVE VISION OF PLACE FOR DOWNTOWN

To achieve the vision of Downtown Westfield, a distinct and clear plan for the zones and places of the Downtown must be created. The result is distinct Plan Areas within Downtown. These Plan Areas control the overall look and feel, the density and architectural requirements, the size and scale and open space, and even the types of developments that are allowed or encouraged in a particular area or place within Downtown.

Individual areas or blocks are highlighted as part of a series of focused vision areas. Those focused vision areas represent distinct planning concepts for their respective areas. To undertake this for every parcel within the Downtown is not only unrealistic but is also doing future designers, developers, and community partners a disservice by taking away their creativity and adaptability. Instead the Plan Areas outline a generalized concept for large areas of Downtown.

Plan Areas vary, from the traditional concept of a Downtown with large buildings and dense developments, to residential neighborhoods with low density and a variety of housing concepts. In-between those extremes, the Redevelopment Plan includes some unique planning concepts specific to the City of Westfield, including development along a new Main Street on SR 32, development specific to the restored Grassy Branch Creekway, and development specific to the Park Street District. Together these Plan Areas represent the full vision of Downtown Westfield and allow the City to set expectations for future development partners.

#### Development Plan Areas

- A. Downtown Core
- B. Main Street
- C. Residential General / Infill
- D. Residential Creekside
- E. Grand Junction District
- F. Legacy and Historic Core
- G. Westfield Boulevard (*Note: Not Included in Downtown, see Part Three*)
- H. Downtown Adjacent (*Note: Not Included in Downtown, see Part Three*)



*Fig. 1.06 - Activated streetscapes and alleys can be vibrant hubs for Downtown, changing from night to day and evolving seasonally to delight visitors.*



*Fig. 1.07 - Urban tree canopy can be a unifying design language across all Plan Areas.*



### PLAN AREA MAP

Shown here are the six Downtown Plan Areas. Typically the boundaries of these plan areas border public right-of-ways, streets, or trails. Areas outside of boundaries shown come under the City Comprehensive Plan jurisdiction. As the City and Downtown evolve over time, the boundaries of these Plan Areas should shift to accommodate development realities, City needs, and an increasing density within Downtown.



Fig 1.08 - Downtown Core Plan Area

## DOWNTOWN CORE

The Downtown core is characterized by a fusion of historic charm, modern appeal, and interconnected public spaces. Two to three story commercial buildings feature retail shops, offices, and small restaurants that complement adjacent residences. New housing includes 3-5 story mixed-use buildings with ground floor retail and residential apartments above. Shared parking garages support the needs of residents and visitors alike. Plazas and other open spaces provide park-like settings, creating a dynamic balance between nature and Downtown. These elements form a cohesive and lively Downtown area where commercial vitality, residential comfort, architectural beauty, and natural tranquility coexist.



Fig 1.09 - Main Street Plan Area

## MAIN STREET

Re-establishing the Downtown main street will create a vibrant core that blends tradition with modern needs. Two to three-story brick buildings frame the street, evoking historic character while providing new opportunity for commercial growth. Public spaces are thoughtfully interspersed, offering plazas and gathering spots that invite community interaction and connectivity. Ground floor restaurants and retail spaces, activate the streetscape while upper levels provide flexible office space. Together, these elements restore main street's role as the civic and cultural heart of Downtown life.

Building facades within the Main Street Plan Area should be of a design and character that contributes to the feeling of a historic Midwest main street. The buildings should include historic character and feature, where possible and practical drawing influence from the history of Westfield, Indiana. Facade materials will be of enduring, timeless materials like brick, stone, glass, metal, and terracotta.



Fig 1.10 - Residential General/Infill Plan Area

## RESIDENTIAL - GENERAL / INFILL

Residential infill areas are envisioned to provide a mix of medium-density residential types, which includes accessory dwelling units, duplexes, triplexes, townhomes, and small scale multi-family. Adding new housing types into existing neighborhoods, a sense of community can be enhanced with variety and character. These new homes support a balanced density and provide a Downtown with a crucial population to support local businesses, opening home ownership and rental to new occupants. Interconnected open spaces and pedestrian pathways connect these areas with Asa Bales Park and the rest of Downtown, integrating recreation and nature into daily life.



Fig 1.11 - Residential Creekside Plan Area

### RESIDENTIAL - CREEKSIDE

The Grassy Branch corridor is a vibrant part of Downtown that blends ecology, housing, and recreation, all focused on the Grassy Branch Creekway. New single-family and multifamily housing along the creekway provide a unique living opportunity in the heart of Downtown. Riparian enhancements restore native plantings, while trails, overlooks, and gathering spaces activate the creek's edge, encouraging recreation and community life. Together, these improvements transform the creek into a central asset that defines Downtown's future.



Fig 1.12 - Grand Junction District Plan Area

### GRAND JUNCTION DISTRICT

The Grand Junction Plan Area is defined by its connection to Grand Junction Plaza and its vibrant restaurant row along Park Street. Existing single-family homes and businesses are interspersed through the area. Additional medium to high density infill housing is anticipated for this area, as are additional 1 to 2 story commercial spaces. Together with Grand Junction Plaza, this forms a charming portion of Downtown life where modern amenities meets hometown charm.



Fig 1.13 - Legacy and Historic Core Plan Area

### LEGACY AND HISTORIC CORE

Focused on the Old City Hall block on Penn Street and the existing housing along North Union Street, this area is intended to retain much of the historical character of Downtown Westfield, focusing on a place that already contains several contributing buildings of the Historic District. If feasible, other historical buildings within Downtown may be relocated to this area. Homes and buildings should retain the historical character of the time that they were built.

The Historic Core is also the beginning of the Westfield History Walk, a waking trail that leads visitors through the history of the City. A public plaza with historical displays will be developed within this area to start this interactive learning experience.

# GRASSY BRANCH CREEKWAY | FOCUSED VISION AREAS

## UNIQUE GREEN PLACES

Gently winding through the heart of Downtown, Grassy Branch is a thoughtfully enhanced and revitalized creekway that creates a seamless connection between people and nature. Once inaccessible, the waterway now becomes an idyllic ecological and recreational corridor.

At the core of the creekway is a creek bed that accommodates substantial water outflow and creates natural flood resilience for Downtown. Crisscrossing that creek are a series of trails, pathways, boardwalks, and promenades. These pathways come in a variety of materials and textures, allowing users of the creekway to experience the creek in new and unique ways each visit. Seating built into the topography of the creek, engineered overlooks, and the pedestrian footbridge all contribute to a feeling of invitation, to sit, to walk, or to run and enjoy the natural world.

Through reintroduction of native wildflowers, grasses, and trees the creekway now attracts pollinators and restores local biodiversity. Restored banks stabilize the soil and filters stormwater runoff, improving the creek's clarity and health.

Development along the creekway should include creative interaction with the natural area. This could include retail tenant spaces that overlook the creek, interesting pathways and connections to the boardwalks, and innovative parking strategies that hide vehicles from the creekway users.



Fig. 1.14 - Grassy Branch Creek Overview Map

### GRASSY BRANCH CREEKWAY | LEGEND

<ul style="list-style-type: none"> <li><b>A</b> Pedestrian Footbridge</li> <li><b>B</b> Elevated Overlook</li> <li><b>C</b> Pedestrian Boardwalks</li> <li><b>D</b> Multi-level Terrace</li> <li><b>E</b> Wetland Enhancement</li> </ul>	<ul style="list-style-type: none"> <li><b>F</b> Native Species Restoration</li> <li><b>G</b> Bank Stabilization</li> <li><b>H</b> Forest Management</li> <li><b>I</b> River Stone Cobble Features</li> <li><b>K</b> Footpaths and Trails</li> </ul>
--	---



Grassy Branch Creekway Site Plan Vision

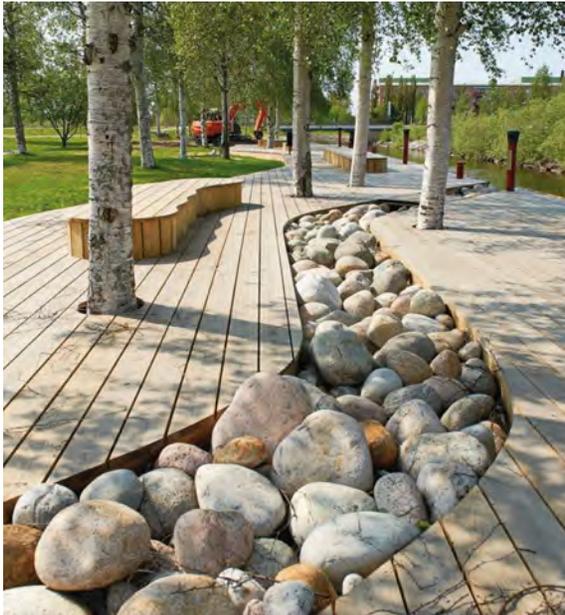


## CREEKWAY VISION

The creekway takes on a unique view depending on how you approach. Arriving from the main north-south pedestrian walkway you are greeted by an iconic footbridge spanning Grassy Branch, providing a crucial pedestrian connection from the north side of Downtown to Midland Trace Trail and Grand Junction Plaza. From the south side, at the historic Old Friends Cemetery Park bridge landing, a unique vista is formed from a view of the creekway, natural design elements, and modern architecture beyond. From the boardwalks and paths within the creekway, it is all natural riparian restoration and waterway. The vision of the creekway is a unique and dynamic visitor experience every day and every season.



*Fig. 1.15 - Elevated overlooks provide a moment of pause on the trail system and an overview of the entire creekway area.*



*Fig. 1.16 - Materials should be used in unique and visually captivating ways that evoke the natural language of the creek.*



## CREEKWAY DESIGN ELEMENTS

In addition to the pathways, boardwalks, and promenades, the creekway has core natural design elements that must be present to ensure a complete design.

Wetland enhancement is planned to reintroduce shallow, vegetated basins along the creek corridor. These wetlands will slow stormwater runoff, improve water quality, and create habitat for wetland-dependent species. Raised boardwalks form pedestrian pathways, with interpretive nodes that guide visitors through the site and offer insights into the area's ecology and history.

The creekway is to be activated through a multi-level terrace wall planted with native species, a naturalistic boulder outcropping, and a scenic trail that connects to Jersey Street. These features blend ecological function with aesthetic appeal, encouraging visitors to engage with the landscape.

To honor the site's heritage, river stone cobble features at key gathering points, echoing Old Friends Cemetery Park's iconic stonework, create a unified and respectful landscape.

## REVITALIZATION

The Grassy Branch Creek Riparian Zone Restoration is envisioned as a multi-faceted project that improves ecological health while creating a welcoming space for the community through the integration of environmental stewardship and thoughtful public design.

### Forest Management

Forest management efforts will focus on removing fallen, broken, and diseased trees to reduce hazards and improve forest structure. A comprehensive tree survey to guide future plantings will ensure a resilient and diverse canopy. Invasive species such as bush honeysuckle and wintercreeper, if present, will be systematically removed, allowing native flora to reestablish and thrive.

### Forest Buffer

A key component of the restoration is the creation of a riparian forest buffer which will stabilize stream banks using native vegetation and natural materials. These buffers reduce erosion, filter pollutants, and provide critical habitat for birds, amphibians, and pollinators. In areas where the creek has been degraded, streambank stabilization techniques—such as coir logs, live staking, and stone toe protection—can be used to reinforce slopes while maintaining a natural appearance.



*Fig. 1.17 - Existing view of Grassy Branch Creek as seen from Union and Jersey Street, looking east.*



*Fig. 1.18 - Existing view of Grassy Branch Creek as seen from the bank of the creek near the Cherry Street Bridge, looking east.*

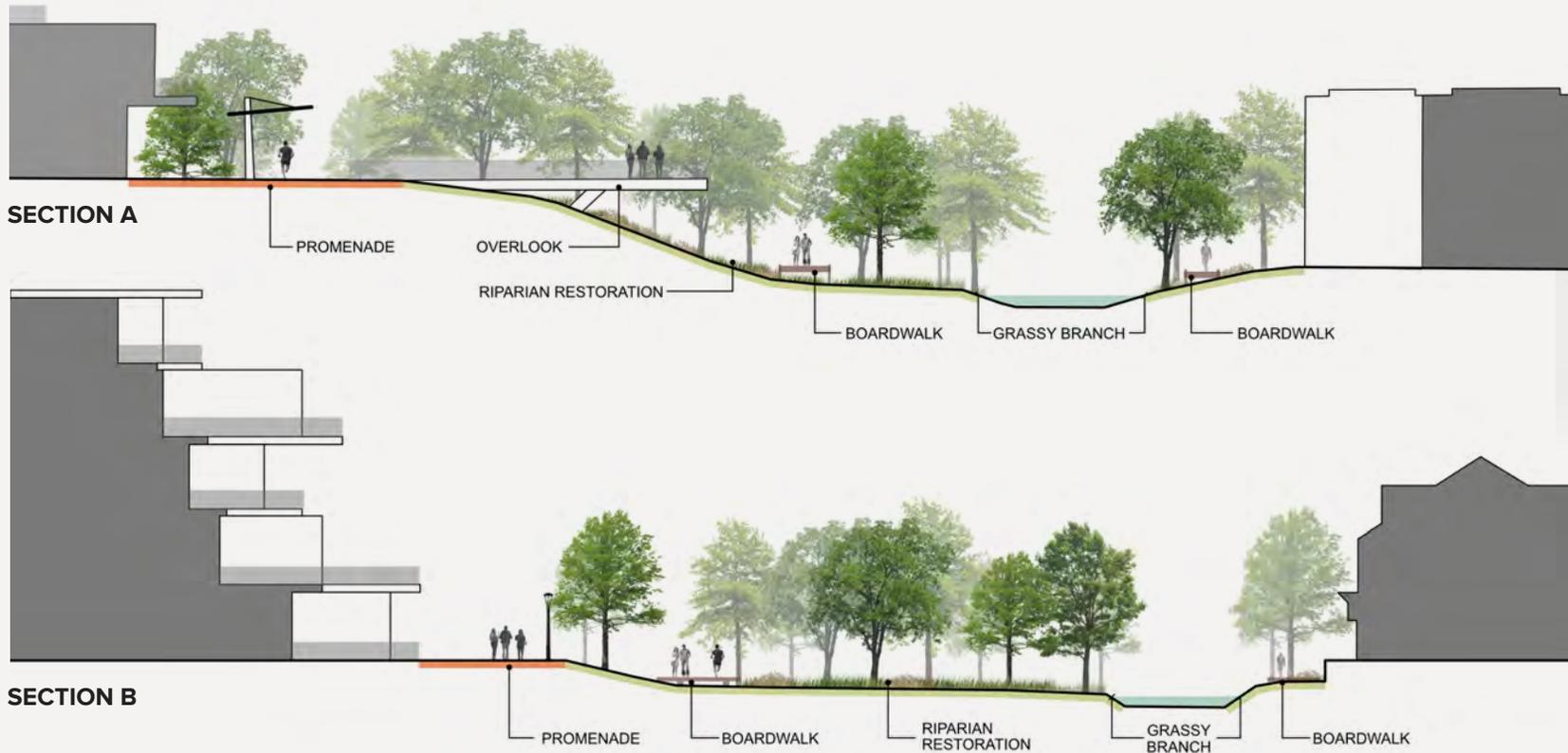


*Fig. 1.19 - The Creekway Vision for the creekbed includes a wider waterway, restored riparian edge, and pathways that lead visitors to the water.*



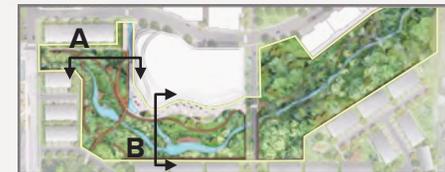
*Fig. 1.20 - The Creekway Vision at the Cherry Street bridge includes an elevated bridge and creekside paths flanking the restored creek edge.*

## GRASSY BRANCH CREEKWAY | SITE SECTIONS



### EXPERIENCING THE CREEK

Visitors to Downtown can experience Grassy Branch in multiple ways. The promenade, an overlook, and pedestrian bridge allows residents and visitors to experience a holistic view of the creekway from above, while boardwalks at the creekside allow people to experience the creek and the riparian forest up close. Educational and interpretative signage promotes education of how best to steward this unique part of Downtown.



# MAIN STREET, WESTFIELD, INDIANA | FOCUSED VISION AREAS

## EMBRACE THE MAIN STREET

SR 32 dominates the central east-west axis of Downtown Westfield. The inclination is to turn your back to the roadway, leaving it to mostly thru-traffic and focusing the design effort on pedestrian-centric spaces located away from the noise, speed, and pollution of the auto traffic. But that misses a key opportunity to redevelop and embrace this central axis of Downtown. The Redevelopment Plan envisions an enhanced SR 32 streetscape that builds on the work underway on the new street widening and setbacks, providing ample pedestrian buffers and amenities and bringing energy back to the Main Street of Westfield.

### Adding to the New SR 32

As of Fall 2025, work is underway on the widening of SR 32; an expanded right-of-way includes 2 travel lanes each direction plus turning lanes, a central tree-lined median, and improvements to the pedestrian way with high quality materials, site furnishings, lighting, and street trees. At between 8 feet and 14 feet on each side depending on the location, this new pedestrian realm is a vast improvement over the previous curb-fronted sidewalk and minimal landscaping. The core challenge of Downtown however remains the same, how does the built-environment embrace Main Street when it's primary use is as a cross-county regional connector road? The vision for Downtown embraces additional setbacks and landscape buffers to achieve this goal. By adding a minimum of 8 additional feet, up to 15 feet, to the setback, a number of features and amenities can be added to the street.



*Fig. 1.21 - Good City design focuses energy and activity on the main street of the Downtown, leaving space for vehicles while providing buffers, slowing traffic through street design, and using high quality materials and landscaping to enhance the experience of the streetscape. Examples of Bad (left) and Good (right) street design.*

### Tree Canopy and Landscape Buffers

The additional setback, in combination with already planned street improvements, provides opportunities for landscape features that provide the safe, comfortable pedestrian environment envisioned for a Main Street like SR 32. Landscape buffers provide shelter for outdoor dining. An additional row of street trees can be accommodated in the wider portions of the roadway. Specialty paving links the inner and outer pedestrian zones.



Main Street (SR 32) Looking West Towards the Union Street Intersection

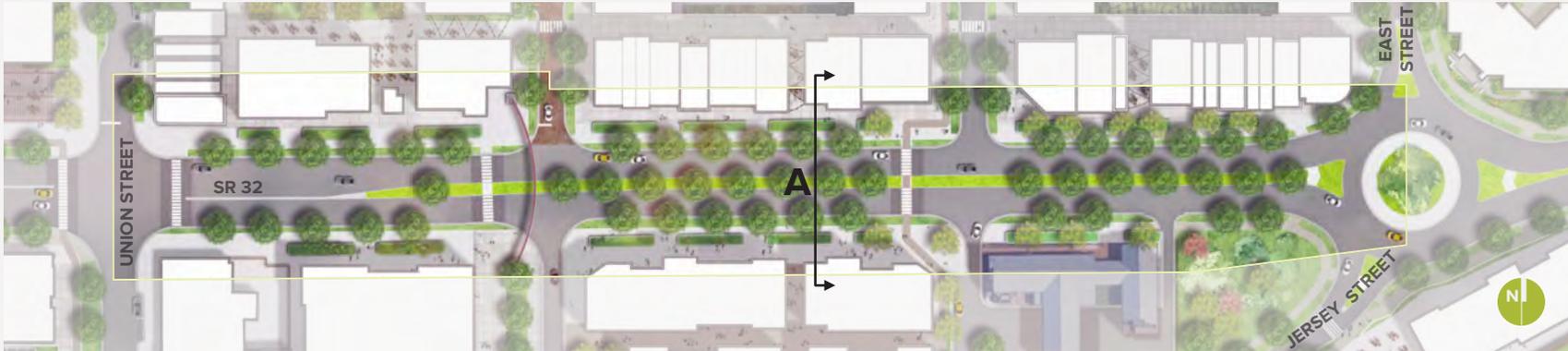
**MAIN STREET WEST | HIGH DENSITY ALONG SR 32**



**HIGHER DENSITY FOCUSED TOWARDS US 31**

West of the central intersection of SR 32 and Union Street is a high density zone of the Downtown Core Plan Area. Buildings are generally 4 to 6 stories with integrated parking garages, large street facades, and retail spaces that are focused on the side streets. Here the right-of-way for SR 32 is generally limited to the planned width of the street improvements due to a combination of existing construction, planned approved development, and utility easements. Activity for the Main Street is intended to be pulled into the side streets via improved streets and plazas, with retail focused where there is easy parking access.

## MAIN STREET EAST | MEDIUM DENSITY ALONG SR 32



### MEDIUM DENSITY STEPS DOWN TO RESIDENTIAL

East of the Union and SR 32 intersection, the density steps down to create a more traditional Midwest Downtown feeling, albeit on a larger street than normal. Within the Main Street Plan Area, buildings are limited to 3 stories in height and are required to have more traditional facade designs and materiality, breaking up the scale of the urban wall and encouraging passive wayfinding. In this area, the right-of-way for the street is enhanced and widened, allowing for diverse and active retail storefronts along the ground floor of the buildings. The pedestrian ways along Main Street connect to a variety of urban trails and plazas through breaks in the buildings.

## BANK BLOCK | FOCUSED VISION AREAS

### THE BUSINESS CORE

Anchoring the corner of Union and SR 32 in the heart of Downtown is the iconic State Bank of Westfield building. This building sets the standard for the architecture of the Main Street Plan Area, which proceeds eastward from the intersection. The entire block takes its name from the corner building and the uses mirror those of the original corner storefronts. The Bank Block is set aside for Office, Professional Services, and General Retail uses within a series of interconnected buildings, all centered around an urban lawn plaza at the heart of the block.

#### Flexible Office Development

The Redevelopment Plan allows for a flexible office development through a series of 3 story office buildings which can be interconnected in a variety of ways to create smaller or larger office floor plates depending on the type of office tenant enticed to the site. Connections at the second or third floor preserve pedestrian pathways that link the adjacent streetscapes to the central plaza. Ground floor tenants can be retail, with direct frontage on the plaza, office, or professional services.

#### Main Street Architecture

Along SR 32, the block is within the Main Street Plan Area. Architecture along that street frontage, and the adjacent Union Street frontage, must conform to the architectural standards of that area. Here though, the 2 and 3 story facades are integrated into the larger office building massing.



*Fig. 1.22 - Historic views of the Bank Building on the northeast corner of Union Street and SR 32 and a prior Bank of Westfield building. The existing building is a key piece of preserved history in the Downtown. Images from Our Westfield 1834-1984*

### THE FRONT LAWN

At the center of the Bank Block is the Westfield Front Lawn urban plaza space. This is envisioned as a large urban lawn where a variety of uses combine for a vibrant, active space. Retail spaces on the ground level open up to the lawn and plaza spaces where food and beverages can be brought out into the lawn and enjoyed in mixed company. General use outdoor dining, urban site furniture, play structures, and whimsical sculptural elements populate the space, letting the lawn be used throughout the year. Overhead lighting, mounted on the buildings, allows for nighttime uses and dynamic lighting shows.

Multiple nodes connect into the plaza, highlighted by the garden path to the west from Union Street and the Carnegie Plaza, the Service Station Building plaza to the south focused on the old service station building, and the Walnut Festival street alley. A connection north to Penn Street completes the access to the lawn from all sides.

## BANK BLOCK | LEGEND

- A** Historic Bank Building
- B** Office Building / Ground Level Retail
- C** Westfield Front Lawn Plaza Space
- D** Historic Service Station Plaza
- E** Walnut Festival Street
- F** Garden Path Connection
- G** Walnut Street Alley
- H** Penn Street Alley
- I** Main Street Architecture Area
- J** Improved SR 32 Streetscape



Bank Block Site Plan Vision



Bank Block Front Lawn Plaza, Viewed from the Office and Retail Buildings along Walnut Street



*Fig. 1.23 - Tree canopy and interesting landscape design frames a central lawn with a variety of seating. Rockville Town Square, Bethesda, Maryland.*



*Fig. 1.24 - The central lawn should be activated at night. Food and beverage service during typical times and special events like Night Markets and seasonal celebrations can provide activations throughout the year. City Place, Spring, Texas.*



*Fig. 1.25 - Pedestrian focused streetscapes can enhance the Downtown and provide dedicated seasonal and special event space right in the heart of Downtown. Festival streets give that flexibility by acting as an enhanced street during normal operation and a special, pedestrian only area during events. West Loop, Chicago, Illinois.*

## **WALNUT FESTIVAL STREET**

Within the Redevelopment Plan, Walnut Street was identified as a potential festival street. This version of Walnut Street, between SR 32 and Penn Street would include decorative street materials such as high-quality brick pavers or stamped concrete to clearly identify the street as special within the Downtown street hierarchy. Enhanced pedestrian connectivity, like raised crossings and zero curb street parking enhance the flexibility of the space. Safety features like retractable bollards allow the full closure of the street for special events, festivals, markets, and other celebrations.

In normal operation, the street functions as a 2-way street with limited street parking and access to the parking garage to the east. In the festival configuration, crash-proof bollards are raised and the street becomes a pedestrian-only zone, allowing for retail activation and block parties. The street includes a direct, short alley connection to the Bank Block Lawn as well, increasing the overall flexibility of the space. From Saturday street markets, to 4<sup>th</sup> of July block parties, the Walnut Festival Street can provide a variety of options for Downtown event planning.

## DOWNTOWN CARNEGIE PLAZA | FOCUSED VISION AREAS

### HEART OF DOWNTOWN

Every City needs a public square. The Carnegie Plaza, located on the northwest corner of SR 32 and Union Street, is that new public square for the City of Westfield. The Carnegie Plaza incorporates the existing historic Carnegie Library, a new public-private office building, and a mixed-use building with retail and apartment living, wrapped around a parking garage. Those buildings frame a series of public plazas and spaces that make up the core of the Carnegie Plaza concept. Shade pergolas, an outdoor amphitheater and lawn space, wide hardscape plazas, and an open path through the public-private office building create a network of pedestrian connections to the rest of Downtown; the circulation heart of the open space network of the City.

In addition to the preservation of the Carnegie Library, the new public-private office building is the other key piece of architecture located on this block. This building should house a premier tenant and should be an icon of architecture in Hamilton County. A separate height allowance for this building is proposed within the Design Guidelines and Standards (Part Three), allowing the building to occupy a key place on the skyline of Downtown. The location of the public-private office building at the heart of Downtown makes the building easily accessible, centrally located to the whole of the City, and a visible icon to citizens and visitors. The building should also support future growth, innovative developments in planning and office spaces, and multi-function and multi-role spaces and uses.



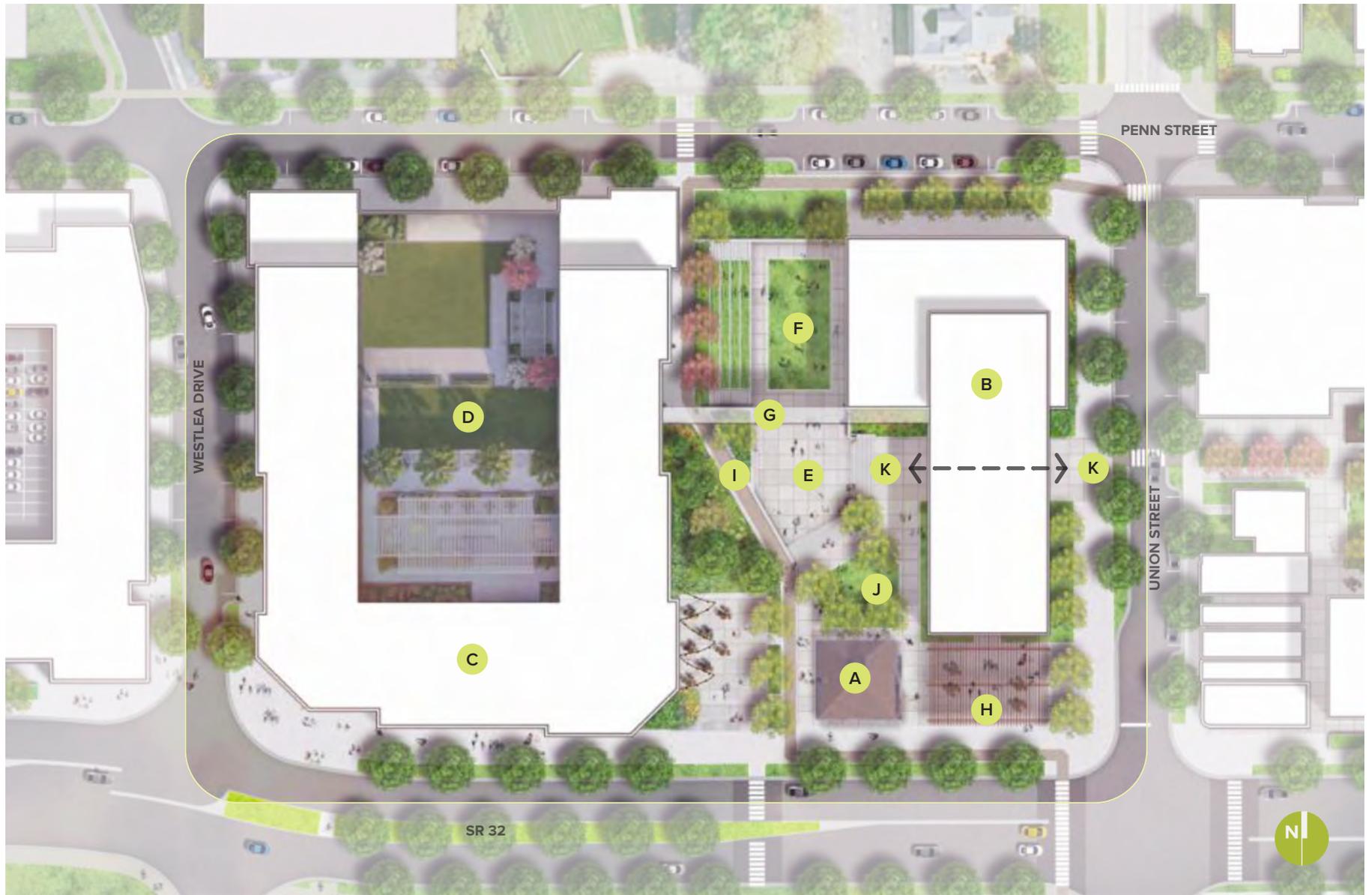
*Fig. 1.26 - Natural elements and plantings tie into modern site materials creating a unique space.*



*Fig. 1.27 - The Public-Private Office Building should open to the plaza, creating an inviting facade.*

### CARNEGIE PLAZA | LEGEND

- A** Carnegie Library
- B** Public-Private Office Building
- C** Mixed-Use Building
- D** Amenity Space and Garage, below
- E** Public Square
- F** Outdoor Amphitheater and Lawn
- G** Bridge to Garage, above
- H** Hadley Garden Plaza
- I** Trail Connection to Asa Bales Park
- J** Historic Garden and Art Installation
- K** Pedestrian Connection under Office Building



Carnegie Plaza Site Plan Vision



*Fig. 1.28 - Timeless materials are used throughout the plaza, enhancing the feeling of permanence.*



*Fig. 1.29 - Natural plants are used in unique and surprising ways to create and subdivide space.*



*Fig. 1.30 - Modern and fun shapes are used to craft outdoor space, including amphitheater seating.*

**A MODERN URBAN PLACE**

The plaza and public spaces framed by the Public-Private Office Building, Carnegie Library, and the Mixed-Use Building form the new public Town Square of Westfield. These spaces are envisioned as modern, stately, and surprising, with wide public hardscape zones of high quality materials like granite blocks and pavers, tiered amphitheater seating opposite the indoor auditorium within the Public-Private Office Building, modern lighted art installations highlighting the history of the Carnegie Library and adjacent land, and natural planting forms.

The public spaces within Carnegie Plaza also form a key missing connection in the fabric of Downtown. Arriving at the south end of Asa Bales Park, pedestrians now have a place to continue on their journey naturally, moving from the green park to the public plaza. Golf carts traveling south on Union Street end their journey there, parking and walking to their final destination. From the plaza, a number of options for connectivity are visible to pedestrians, including paths down Penn Street, to the intersection crossing at Union and SR 32, and east to the Bank Block Front Lawn.



*Fig. 1-31 - Art should be modern and clean, juxtaposing the historic nature of the Library Building.*



Fig. 1.32 - North edge of the Penn Street Extension should engage with the residential buildings.



Fig. 1.34 - Extension of Penn Street West of Union Street, Phasing Approach



Fig. 1.33 - Where present, parking garage facades should be screened, preferably with greenery or facades which complement surrounding architecture.

## EXTENDING PENN STREET

Currently, Penn Street ends at the east curb of Union Street. Throughout the Redevelopment Plan, major streets have been preserved or identified as part of the City grid, keeping a visual language easily understandable to pedestrian, bicyclist, and passenger vehicle driver alike. Penn Street currently breaks that convention. A key portion of grid preservation and restoration is the extension of Penn Street west, to meet with Westfield Boulevard / Shamrock Boulevard. The extension allows for major development to occur in key Plan Areas, and for low-speed vehicular access and street parking supporting businesses on the north side of SR 32 that are cut off from half of the traffic flow of that street due to the street realignment.

Recognizing that the extension of Penn Street is a complex process, the extension project is proposed in two phases. Phase one would be constructed with the Carnegie Plaza Catalyst Project (see Part Two). Phase One connects Penn Street between Union and Westlea Drive, completing a loop and allowing traffic to access the Garage in the Mixed-Use Building from a variety of directions. Phase two completes the City grid, connecting Westlea Drive to Westfield Boulevard, aligning with the access drive to the hospital. The phase two work would likely be timed with the end-of-life of the existing school tennis courts to facilitate easier construction.

Penn Street (both new extension and existing eastern section) represents a key transitional street in the Redevelopment Plan. At Penn Street, development shifts from the Downtown Core to Residential and Historic Plan Areas. This shift requires a step-down in building height with a maximum of three stories on each side. On the south side that is accomplished by a required step-back in the building height at the street. On the north edge, it is a maximum height requirement. The result is an easy transition in building height and massing as the density of Downtown steps down between Plan Areas.

## DOWNTOWN HISTORY WALK | FOCUSED VISION AREAS

### WALK THE HISTORY OF WESTFIELD

From the pioneer spirit of settlement, to a key service stop in the nascent personal automobile culture and beyond, the history of Westfield is rich and varied. The City even played a key role in the central route of the Underground Railroad. But time and evolving demographics have eroded what was once cultural memory. The Downtown History Walk seeks to educate and revive this history through active and passive urban storytelling.

#### Historic Block

Located adjacent to the historic City Hall and Barker Family Cabin, the Block serves as a central node for places and spaces of historic importance. Empty parcels near the Block serve as future landing spaces for historic buildings that may be moved as part of the Redevelopment Plan for Downtown.

#### History Walk and Walk Nodes

A History Walk guides residents and curious visitors along a path that explores the rich history of Westfield. The Walk starts at the Grand Junction pavilion, with wayfinding signage guiding people along the path before completing the journey at the Historic Block.

Sculptures, art, and other interpretive and historical markers are to be incorporated at key moments along the history walk. These highlight important historical moments and spaces throughout the Downtown, including the Carnegie Library, Bank Building, former Fire Station, and other appropriate moments of importance and significance to the History of the City.



Preliminary Routing for History Walk



Fig. 1.35 - The History Walk should include a variety of passive storytelling elements within the urban environment like the plaques and text in this sidewalk trail. Visitors should be able to pick up the story at any point in their journey.



Fig. 1.36 - At key moments, art and sculpture can be used to enhance the story of the history of the place.



Fig. 1.37 - An example of an outdoor history wall exhibit at the Josiah Henson Museum in Maryland.

### INTERACTIVE HISTORY WALL

Located just a five-minute walk from the Historical Society, an interactive history wall anchors a pocket plaza along the History Walk. Rooted in a landscape once shaped by agriculture and horse farming, the installation honors the people—enslaved and free, Black and white—who cultivated the soil, sheltered freedom seekers, and sowed the seeds of justice and knowledge.

Through symbolic forms and layered storytelling, the piece reflects the region’s agricultural heritage, the equestrian traditions that supported rural life, and the faith-driven resistance of Quaker communities who played a vital role in the Underground Railroad. It also celebrates the transformative power of education as a pathway to freedom and self-determination.

This installation invites reflection on how ordinary people, guided by conscience and courage, used the resources of the land—barns, horses, fields, and faith—to create extraordinary change. It stands as a tribute to resilience, quiet heroism, and the belief that liberation begins with both action and understanding.

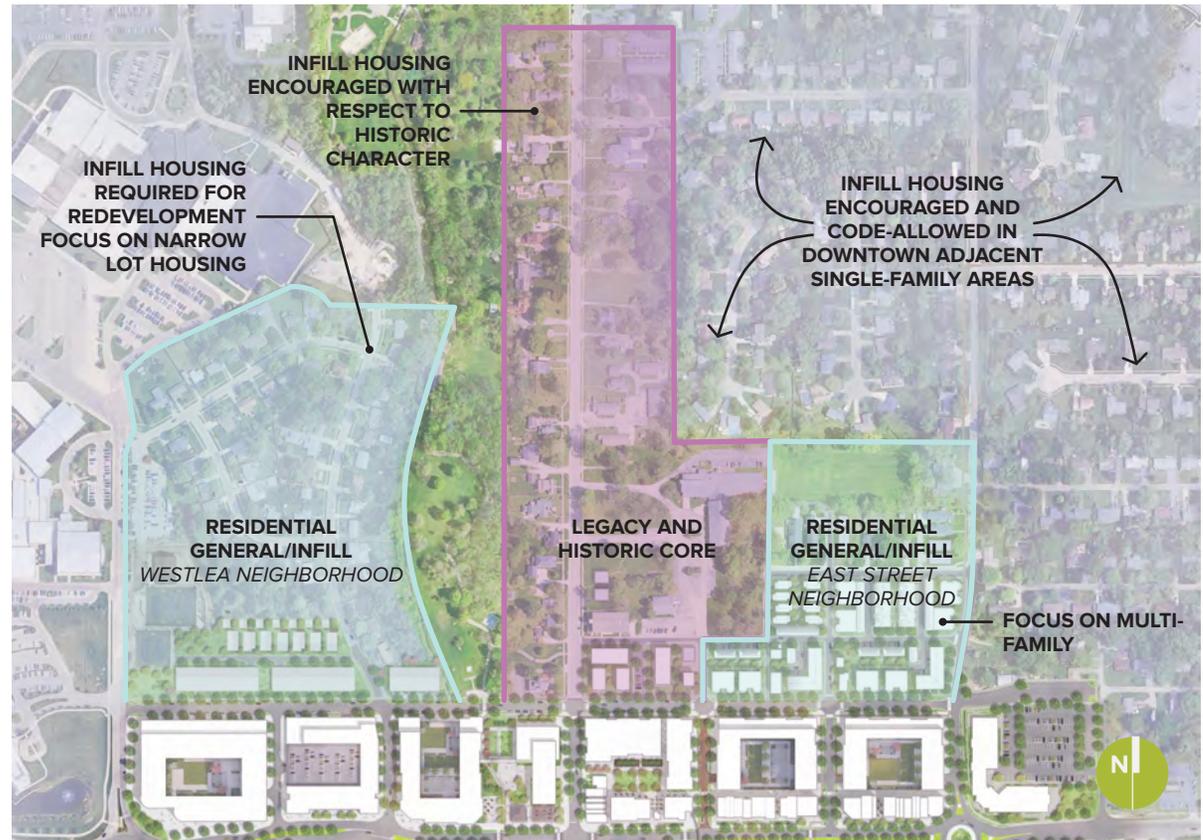
## INFILL HOUSING | FOCUSED VISION AREAS



Fig. 1.38 - Infill housing can be charming and beautiful, enhancing the street and providing for a diverse urban fabric.

### HOUSING IN DOWNTOWN

Westfield is undergoing a transformation from a City focused primarily on single-family developments to one with an expanding and more varied housing base to accommodate a growing population. To date that variation (both in practice and planning) has been primarily in multi-family developments focused on apartment living, combined with higher end townhouses. The vision for Downtown is one that encompasses a full range of housing options, from narrow lot single-family detached, to duplexes, triplexes, and quadplexes, from townhouses to small-scale multi-family developments. In the Downtown Core Plan Area, large scale multi-family mixed-use projects are contemplated. But the surrounding Plan Areas encourage a wider variety of housing stock. Infill housing is a key component of this transformation.



### Housing by Plan Area

Infill housing takes a variety of forms, from the simple increase in density allowed for Accessory Dwelling Units on former single-family lots, to the more complex forms of multi-unit buildings and even small scale multi-family developments with 4-20 units. In the Downtown Plan Areas identified above, additional density via infill housing is required for redevelopment of existing parcels. In the areas surrounding Downtown, the use of infill principles is encouraged and allowed by code, both in this document and in the City Comprehensive Plan.



Fig. 1.39 - Small Lot Housing Prototypes. City of Houston.



Fig. 1.42 - Narrow Lot Single-Family Houses



Fig. 1.43 - Narrow Lot Single-Family Houses



Fig. 1.40 - Duplex, Triplex, or Stacked Flats multi-family housing should be used to transition between single-family areas and more dense urban areas.



Fig. 1.41 - Townhouses are suitable for a variety of urban areas and can act as transitional zones or stand-alone as featured housing in dense areas.



Fig. 1.44 - Small-scale multi-family developments (4-20 units) can be very effective workforce housing options and infill larger lots and parcels.

## WELCOME TO WESTFIELD | FOCUSED VISION AREAS

### WHAT IS THE IDENTITY OF DOWNTOWN WESTFIELD?

Downtown should have a unique place within the fabric of a City. Not elevated or above, but singular within the urban places that make up a modern City. That identity can be achieved through a variety of tactics and strategies. Architectural character, quality of public place, and centralized nodes of civic and business functions (think the classic courthouse square) can all identify a place clearly and distinctly as the center of the City. The Redevelopment Plan contains many opportunities to create that sense of place. Most notably in the Main Street Plan Area and in the diverse stitching of public spaces, trails, and pathways.

Layered on top of the vision for the Downtown built environment is a network of art, sculpture, and signage that brands the Downtown. These elements can take varying forms, and should tie into a broader Art Plan for the entire City, but should remain special within that plan. Things like City signage should be tailored to Downtown with a special design scheme or identifying symbols. Color can play a role too, with different districts within Downtown using unique colors within an overall theme to distinguish the different places.

New visitors to Westfield should be able to instantly identify they are in the heart of the City. The extra layer of branding and identity through environmental art and signage is what embodies that “Welcome to Westfield” moment, creating a lasting impression on citizens and visitors alike.

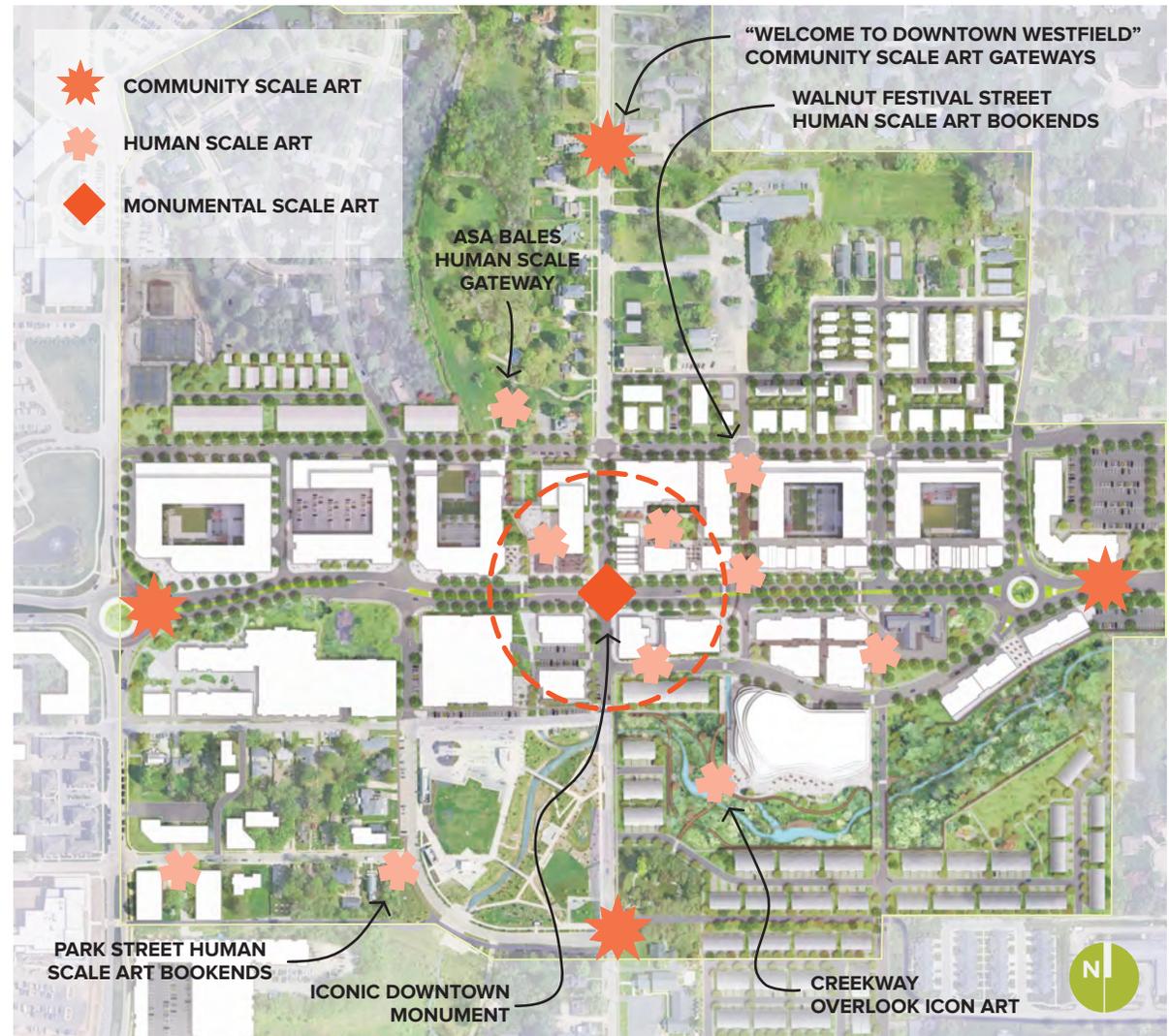


Fig. 1.45 - Downtown Identity: Downtown Gateway and Public Space Art Opportunities



Fig. 1.46 - Community-Scale art installations can act as visual gateways to Downtown. Large scale sculpture can be interactive and stitched into the urban design of the plazas and spaces where they are placed.



Fig. 1.47 - Signage and Art can go hand-in-hand with small Human-Scale installations acting as interactive opportunities and branding for Downtown.



Fig. 1.48 - View of a possible Carnegie Plaza Interactive Art Installation

## GATEWAYS, BOOKENDS, AND MONUMENTS

Within the Redevelopment Plan, there are several categories of environmental art and sculpture. The categories help identify the goal of the art, the general placement, and in the case of the monument, a historic or cultural significance. Environmental art and sculpture can vary in size and scale. They range from Community-Scale installations, which are large and often integrated into the design of open space, to Human-Scale installations which are smaller, singular pieces which can be interacted with or observed by visitors to a space.

### Gateways

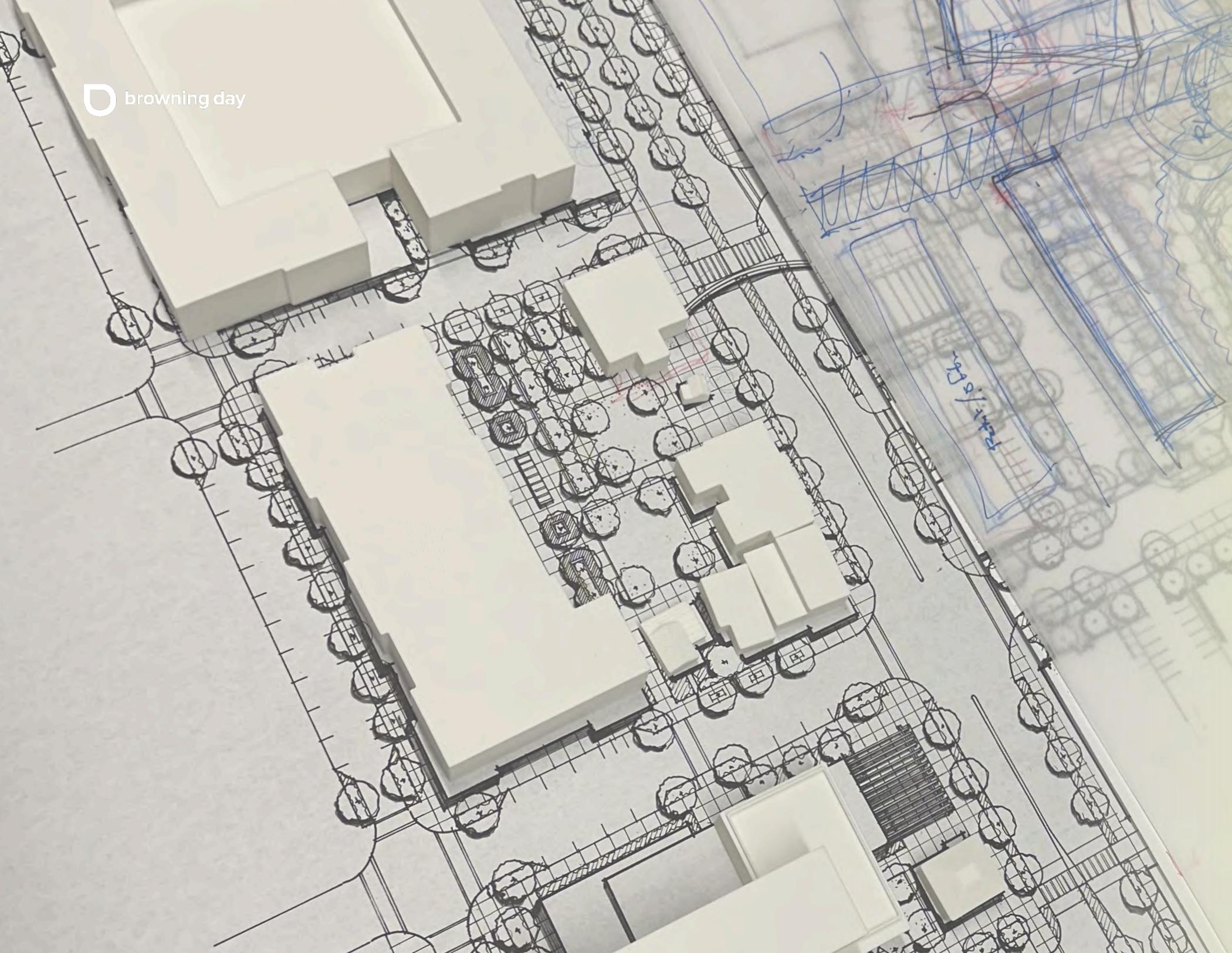
Gateways are art opportunities that serve as markers of entry into Downtown. Positioned at the key arrival points (by car, on foot or golf cart) these identify you are entering Downtown Westfield.

### Bookends

Bookends are specific to key streets and/or trails. They mark each end of a special place and should be developed as a pair, complimentary to each other. Park Street and Walnut Street are streets which include bookends.

### Monuments

In Westfield, this central and iconic orienting monument should pay homage to the unique culture and history of the place, while leading the City forward into a new age. Because of its symbolic ties to Westfield's history, the Lantern can play a role in the design of the installation as seen above.





# TWO

**VISION IN ACTION**  
IMPLEMENTATION & PHASING

## IMPLEMENTATION & PHASING | OVERALL DOWNTOWN DEVELOPMENT

### DEVELOPMENT & PHASING

Redevelopment of downtown is expected to occur in many phases over the next few decades.

Thoughtful phasing of development is essential to ensure projects unfold in a logical manner that builds momentum over time. A clear plan aligns infrastructure, public spaces, and private investment, creating a connected and intentional outcome rather than piecemeal growth. Early phases can not only build momentum but also allow the community time to evaluate progress and modify the plan as needed to ensure long term viability and success.

The phasing for the downtown redevelopment vision is anticipated to occur over the next 2+ decades. First, Catalyst Projects will jump start overall development and energy downtown, creating excitement and investment towards the long term vision. Key Projects are anticipated to be interspersed throughout the early phases, aligning infrastructure, public spaces, and key parcels for development. Horizon Projects, like infill housing and projects in downtown adjacent areas, are anticipated to occur intermittently over the next 20+ years as the demand for housing continues to increase.

This thoughtful plan for implementation will require consistent coordination and collaboration between multiple municipal departments, commissions, boards, and developers to bring the vision to life. An implementation schedule is to be maintained to ensure this collaboration occurs, and to make adjustments to the plan if necessary.



## DEVELOPMENT SUMMARY

### Residential Units

Total Units: +/- 1,695 units

Single-Family Detached: 112+ units

Single-Family Attached: 146 units

Multi-Family: 1,437 units

### Office Space

Total Square Footage: +/- 297,236 sf

### Retail Space

Total Square Footage: +/- 362,426 sf

### Parking

Total Parking Spaces: +/- 4,314 spaces

Garage: +/- 3,157 spaces

Surface Lot: +/- 310 spaces

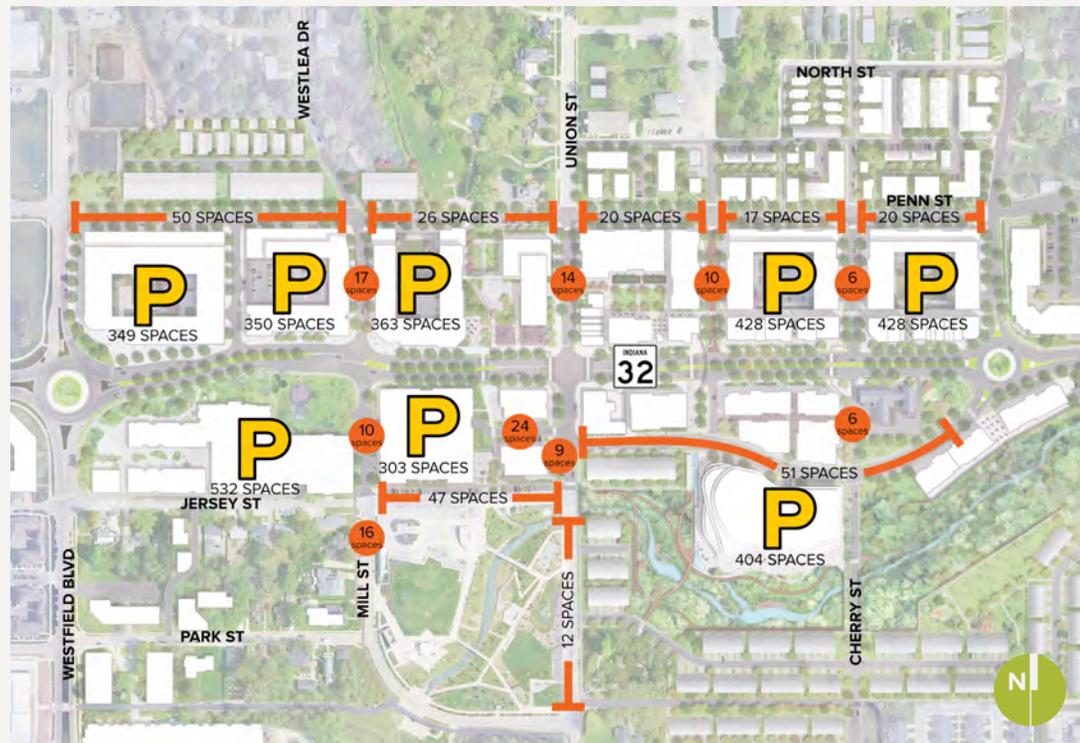
Street Parking: +/- 331 spaces

Private Single-Family Residential: 516 spaces

### Notes

- Counts are estimates based on typical building layout, square footage, and unit counts.
- Private Single-Family parking counts assumes two off-street spaces per unit.
- Parking is anticipated to be shared throughout the day. For example, Person A may live downtown and use a street parking space overnight and on weekends, while Person B works downtown and may use that same space during the work day. Additionally, retail and restaurant users will use these spaces throughout the day, and be willing to walk a couple of blocks to their destination.

## PARKING | DOWNTOWN CORE STREET & GARAGE COUNTS



NOTE: Parking counts are estimated based on typical building layout and size. Parking shown is sufficient for the Redevelopment Plan as envisioned.

# IMPLEMENTATION & PHASING | OVERALL DOWNTOWN DEVELOPMENT

## PHASING

Catalyst, Key, and Horizon projects have been planned to spark development, maintain momentum, and build out a downtown to last generations. Generally, it is expected that construction for the Redevelopment of Downtown will begin in Fall of 2025 and continue for the next 20+ years.

### Catalyst Projects

These projects are essential to creating and building momentum for downtown redevelopment, and form critical pieces of the overall vision. These projects should occur first or within the first 4 years.

### Key Projects

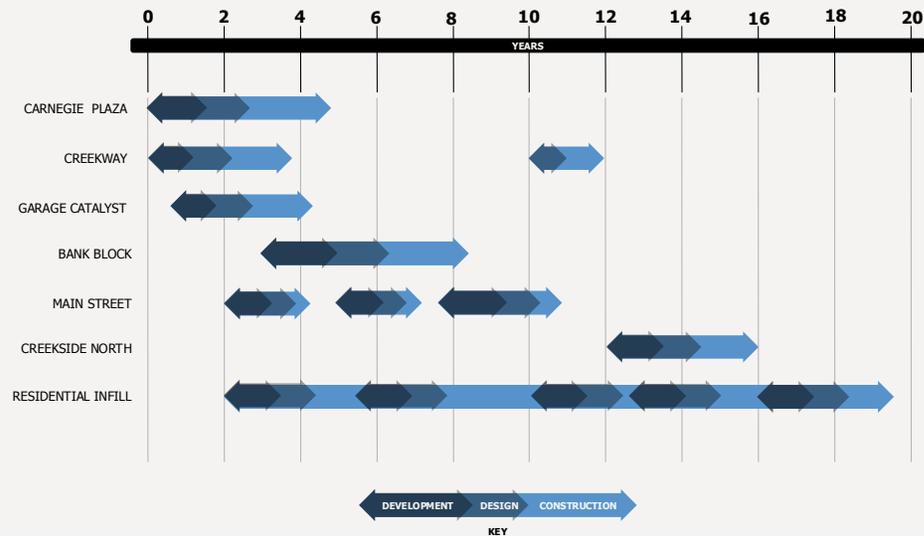
Key projects are those that play a fundamental part of the downtown vision. These may include specific developable lots, public spaces, or crucial infrastructure projects. Without these projects the overall vision may be diluted. Ideally these occur within the first 8 years, but may extend beyond that timeframe as needed.

### Horizon Projects

Horizon Projects encompass other redevelopments that are not Catalyst or Key Projects. These may occur at any time depending on market conditions. They primarily play a supporting role for the downtown by providing housing or additional commercial space.

## ANTICIPATED PHASING | NEXT 20+ YEARS

Below is an anticipated phasing schedule for Catalyst, Key, Horizon, and other Projects as part of the Redevelopment Plan. While not all potential projects are shown, the ones below are representative of the overall Plan for Downtown. Catalyst Projects are anticipated to begin first, while some Key Projects - like the Creekway - may also begin in early years. Projects like the Main Street represent several separate projects that may occur over time, and the Residential Infill represents infill projects that will generally be ongoing.

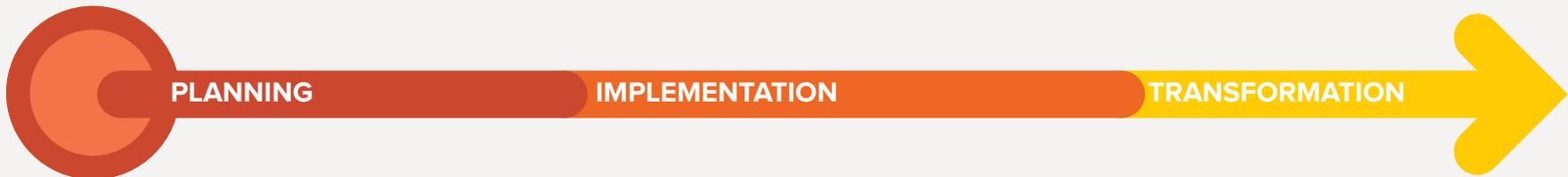


### NOTES

- Development includes land acquisition, due diligence, concept design, design development, economic development introductions and other research.
- Design includes construction documentation, bidding, procurement and permitting.
- Construction includes contracting, materials procurement, closeout and commissioning.

## DEVELOPMENT MILESTONES | NEXT 20+ YEARS

Setting milestones and laying out a plan for hitting those milestones is crucial for the success of Downtown. The following milestones are grouped in 3 tiers of general timing. Some may happen earlier or later than anticipated - others may overlap - but generally this is a framework for completing the Redevelopment Plan.



### NEAR-TERM

**Focus**

Planning, Communication, Groundwork

**Purpose**

Set the foundation for redevelopment.

- A. Downtown Redevelopment Plan.
- B. Develop budgets for capital plans.
- C. Zoning changes or approvals.
- D. Crucial infrastructure projects.
- E. Temporary site activation (e.g. pop-ups, road diets, art installations).
- F. Grassy Branch woodland management and trails.
- G. Construction of Mixed-Use projects with structured parking.

### MID-TERM

**Focus**

Construction, Infrastructure, and Early Activation

**Purpose**

Begin visible transformation and build momentum.

- A. Major infrastructure upgrades.
- B. Launch of anchor tenants or civic facilities.
- C. Streetscape improvements and trail connections.
- D. Public space creation and enhancements.

**NOTES**

- Milestones are based on current downtown development and market conditions.
- Development milestones should be routinely reviewed - and revised if needed - based on the Plan Maintenance Schedule.

### LONG-TERM

**Focus**

Full transformation and planning maintenance.

**Purpose**

Deliver lasting impact and community value.

- A. Completion of full redevelopment phases.
- B. Economic revitalization (e.g. continued job creation and business growth).
- C. Long-term housing stability and affordability metrics.
- D. Street connections and other infrastructure projects.
- E. Ongoing public open space stewardship.

## IMPLEMENTATION & PHASING | OVERALL DOWNTOWN DEVELOPMENT

- 01 Mixed-Use Residential
- 02 Office + Garage
- 03 Mixed-Use Residential/Office
- 04 Public-Private Office Building
- 05 Existing Bank Building, Reuse
- 06 Office - South
- 07 Public Plaza
- 08 Office - Northwest
- 09 Office - Northeast
- 10 Retail Main Street
- 11 Residential Multi-Family
- 12 Retail Main Street
- 13 Retail Main Street
- 14 Residential Multi-Family
- 15 Residential Multi-Family
- 16 Ambrose on Main - (Planned)
- 17 Retail Main Street
- 18 Retail Main Street
- 19 Retail Main Street
- 20 Jersey Street Retail
- 21 Jersey Street Retail
- 22 Retail (Existing)
- 23 Field Brewing (Existing)
- 24 Residential Multi-Family
- 25 Townhomes



## IMPLEMENTATION & PHASING | DEVELOPMENT DETAILS

Area	Key	Importance	Building Type	Building Location	Parcel Ownership	Stories	Total Bldg SF (est.)	Units	Site Area SF	Parking Spaces	Street Parking
<b>W. Penn St. Block</b>											
	01		Mixed-Use Residential	SR 32 E and Westfield Blvd. Roundabout; NE Corner		4	232,759	194	127,400	--	26
	01-R		Mixed-Use Retail (Penn Extension facing)	SR 32 E and Westfield Blvd. Roundabout; NE Corner		1	22,450	--	--	--	--
	01-G		Mixed-Use Garage	SR 32 E and Westfield Blvd. Roundabout; NE Corner		3	166,500	--	--	349	--
	02		Office (and Garage)	SR 32 E and Westlea St; NW Corner		4	185,912	--	80,414	350	31
<b>Carnegie Plaza Block</b>											
	03	Catalyst	Mixed-Use Residential	SR 32 E and Westlea St; NE Corner		5	145,718	121	82,140	--	25
	03-R	Catalyst	Mixed-Use Retail	SR 32 E and Westlea St; NE Corner		1	23,023	--	--	--	--
	03-G	Catalyst	Garage	SR 32 E and Westlea St; NE Corner		4	104,367	--	--	363	--
	04	Catalyst	Public-Private Office Building	SR 32 E and Union Street; NW Corner		5	75,000	--	82,650	--	14
	04-P	Key	Public Plaza	Mid-Block, SR 32 E to W Penn St. Extension					46,165		
<b>Bank Block</b>											
	05		Existing, Bank Building Reuse	SR 32 E and Union St; NE Corner		2	7,870	--	11,785	--	4
	06		Office-S	SR 32 E East of Union; Mid-Block to Walnut St.		3	27,570	--	18,640	--	--
	06-R		Office-S, Retail Tenant	SR 32 E East of Union; Mid-Block to Walnut St.		1	13,785	--	--	--	--
	07	Catalyst	Public Plaza	Center of Block					22,645		
	08		Office-NW	E Penn St. and Union St; SE Corner		3	54,222	--	29,360	--	12
	08-R		Office-NW, Retail Tenant	E Penn St. and Union St; SE Corner		1	15,300	--	--	--	--
	09		Office-NE	E Penn St. and Walnut St; SW Corner		3	29,532	--	16,830	--	9
	09-R		Office-NE, Retail Tenant	E Penn St. and Walnut St; SW Corner		1	14,766	--	--	--	--
<b>Main St. North Block</b>											
	10		Retail Main Street 1	SR 32 E and Walnut St; NW Corner		2-3	25,797	--	11,400	--	--
	11		Retail Main Street 2	SR 32 E and Cherry St; NE Corner		2	13,456	--	7,400	--	--
	12		Retail Main Street 3	SR 32 E and Cherry St; NW Corner		2	12,426	--	7,100	--	--
	13		Retail Main Street 4	SR 32 E and East St. Roundabout; NW Corner		2	21,572	--	11,730	--	--
	14		Mixed-Use Multifamily	E Penn St. and Cherry St; SE Corner		4	96,900	81	65,435	--	17
	14-R		Mixed-Use Retail	E Penn St. and Cherry St; SE Corner, Cherry/Walnut Facing		1	15,600	--	--	--	--
	14-G		Garage 1	E Penn St. and Cherry St; SE Corner		5	124,875	--	--	428	--
	15	Catalyst	Mixed-Use Multifamily	E Penn St. and Cherry St; SW Corner		4	96,900	81	65,845	--	14
	15-R	Catalyst	Mixed-Use Retail	E Penn St. and Cherry St; SW Corner, Cherry/East Facing		1	15,600	--	--	--	--
	15-G	Catalyst	Garage 2	E Penn St. and Cherry St; SW Corner		5	124,875	--	--	428	--
	16	Catalyst	[Planned] Ambrose on Main	SR 32 E and East St. Roundabout; NE Corner		4	119,500	87	119,710	184	--
<b>Main St. South</b>											
	17		Retail Main Street 1	SR 32 E and Walnut St; SW Corner		2	26,802	--	21,600	--	6
	18		Retail Main Street 2	SR 32 E and Walnut St; SE Corner		2-3	21,690	--	14,950	--	--
	19		Retail Main Street 3	SR 32 E and Cherry St; SW Corner		2	13,338	--	8,700	--	2
	20		Jersey Street Retail 4	Jersey St. and Walnut St; NE Corner		2	24,988	--	13,565	--	8
	21		Jersey Street Retail 5	Jersey St. and Cherry St; NW Corner		2-3	24,540	--	13,750	--	9
	22		[Existing] Retail	SR 32 E and Union St; SE Corner		1	9,507	--	--	--	8
	23		[Existing] Field Brewing	SR 32 E and Cherry, SE Corner		1	10,458	--	--	--	14
<b>Creekside North</b>											
	24		Mixed-Use Multifamily	Jersey St. and Cherry St; SW Corner		5	163,014	136	99,350	--	12
	24-R		Mixed-Use Retail	Jersey St. and Cherry St; SW Corner		1	15,000	--	--	--	--
	24-G		Mixed-Use Garage 1	Jersey St. and Cherry St; SW Corner		4	159,500	--	--	404	--
	25		High-End Townhouse Podium	Union St and Jersey St; SE Corner		3	42,000	19	21,700	20	--
	26	Key	Trail and riparian enhancements	Creekside, multiple routes					134,285	--	--
	27	Key	Promenade and pedestrian bridge	North/south connector from Midland Trace Trail to Jersey Street					--	--	--

## IMPLEMENTATION & PHASING | CATALYST PROJECTS

### CATALYST PROJECTS

As an essential part of the redevelopment process, Catalyst Projects are responsible for creating a spark that drives development and economic momentum for future phases of the long term vision. Catalyst projects represent visible progress, and are important in building public support for continued growth and development.

For downtown Westfield, these Catalyst Projects can be sorted into a few projects, each providing a key piece of the downtown vision. Their success builds the foundation for future investment into downtown.

#### Carnegie Plaza Block

In the heart of downtown, the Carnegie Plaza Block builds a new town square for the future, with investment in public spaces open to all. Office spaces bring regular use to downtown, and the regular presence of workers can bolster nearby business use. Retail spaces on the ground floor of adjacent buildings, urban site furniture, and play elements unite to create a vibrant community space with year-round engagement.

#### Garage Integration

Parking is a crucial part of downtown success, especially early in the redevelopment process. Though early projects may be served by surface lots, the density and vision for downtown will require several publicly available garages to be constructed. These are likely to be integrated with other residential, commercial, or mixed-use projects.

### CATALYST PROJECTS | MAPPED LOCATIONS



## CATALYST PROJECTS | CARNEGIE PLAZA BLOCK

### DESIGN INTENT

A public-private office building frames a new community oriented space, creating a vibrant destination at the heart of downtown. A plaza, which links Asa Bales Park to downtown, anchors the block and is designed as an extension of the office building itself - an adaptable gathering space that can host entertainment and other events throughout the year. A public garage, wrapped with retail and residential uses, supports the block. Together, these elements create a cohesive Downtown destination.



*Office Building and Public Plaza*



*Mixed-Use Building with Garage and Courtyard*



*Public Garage Wrapped by a Mixed-Use Building*

### OFFICE BUILDING & PLAZA

A home for community engagement, the public/private office building is a landmark building for downtown. It is programmed for offices, reception space, and community spaces. A plaza extends these spaces, providing a gathering place for downtown visitors.

Building: 5 stories, 75,000 sf

Plaza: 46,000 sf

### MIXED USE

A 5 story mixed use building with ground floor retail space and residential units above. All these uses are wrapped around a public garage.

Retail: 23,000 sf

Residential Units: 121 apartments

### GARAGE

To support activity generated by the office building a public parking garage has been incorporated into a mixed-use building that includes retail and residential uses.

Total Parking Spaces: 363 spaces

## CATALYST PROJECTS | GARAGE INTEGRATION

### DESIGN INTENT

Parking plays a crucial role in the success and functionality of Downtown. In particular, structured parking efficiently uses and preserves valuable land – allowing Downtown to develop with an appropriate and economically viable density of residential, commercial, and office uses. Several garages are envisioned for the area, with most having additional parking available to the public to support adjacent commercial and office uses. These garages increase accessibility for Downtown visitors, stimulate investment by providing necessary infrastructure, and promote a walkable Downtown by consolidating parking. With this strategy Westfield can balance mobility needs with placemaking and economic growth.



*4 Story Mixed-Use Building with Garage*



*4 Story Mixed-Use Building with Garage*



*5 Story Mixed-Use Building with Garage*

### PENN & WALNUT STREET GARAGE

This garage is envisioned to be integrated within an mixed-use building. A key aspect of this garage is that it provides parking for the adjacent Bank Block to the west. Without this garage the Bank Block cannot be developed as envisioned.

Garage: 343 spaces across 4 levels.

### PENN & CHERRY STREET GARAGE

This garage, along with its sister garage at Penn and Walnut Streets, provides crucial public parking for the north side of Downtown. These garages support their integrated residential units along with adjacent retail and office uses.

Garage: 343 spaces across 4 levels.

### CREEKSIDE GARAGE

This garage, located between Jersey Street and Grassy Branch, is specially located and sized to provide public parking for the parts of Downtown south of SR 32.

Garage: 404 spaces across 5 levels.

## IMPLEMENTATION & PHASING | KEY PROJECTS

### KEY PROJECTS

Key Projects are essential to the conceptual essence of the long term downtown vision. Without these Key Projects, the overall concept may quickly be diluted. These projects are primarily focused on public spaces that perform an important role in stitching together Catalyst Projects and forming a foundation for the overall vision.

While they may not be the first projects completed for downtown redevelopment, Key Projects are anticipated to be completed within the early phases of overall redevelopment. Some Key Projects can be started immediately while others are reliant on completion of Catalyst Projects.

Due to the proximity and location within the floodplain, it should be anticipated that these projects have long permitting timelines, specifically with DNR, IDEM, and other similar agencies.

### KEY PROJECTS | MAPPED LOCATIONS



## KEY PROJECTS | BANK BLOCK

### DESIGN INTENT

Preservation and economic development work hand-in-hand for the Bank Block, so named after the historic bank building at Union Street and SR 32, as existing buildings are renovated/restored and new mixed-use buildings provide economic vitality for the downtown. The block provides space for multi-tenant office buildings and/or a corporate headquarters, while promoting retail uses with historic charm along the main street. The historic filling station is relocated and reused on this block to make space for new development while retaining a unique element of Downtown history. All of this is stitched together with a central plaza.



*Corporate Headquarters or Multi-Tenant Office*



*Main Street Retail and Existing Bank Block*



*Plaza Stitching the Block Together*

### OFFICE

A prime location for a corporate office or a multi-tenant office building. The offices envisioned on this block play a key role in creating round the clock activity for downtown by providing a consistent source of people coming to the downtown area. Building floor plates can be connected on the second or third floor as needed.

Approximately 83,500 sf of office space across 2 three story buildings.

### RETAIL

Restoring and renovating existing buildings on this block plays a key role in its development. These buildings form the core of the historic charm on SR 32. New retail buildings are integrated with the old, and the historic fueling station building is relocated on the block for a new use.

Approximately 44,000 sf of new retail space. Buildings are 2 to 3 stories.

### PUBLIC PLAZA

A public plaza stitches this block together. Providing a relaxing outdoor space for nearby workers and downtown visitors alike. The plaza includes seating, landscape areas, informal play structures, and may be programmed for specific activities or events.

Approximately 23,000 sf

## KEY PROJECTS | PUBLIC SPACE ENHANCEMENTS

### DESIGN INTENT

The enhancement of Grassy Branch plays a key role in downtown redevelopment. Not only does it stitch together parts of downtown that were previously separated, it also restores the riparian environment along the creek side. This enhancement includes removing invasive species and restoring native vegetation along the creek banks, and thoughtfully building trails alongside the creek to promote outdoor recreation and a connection to nature. A promenade provides retail opportunities overlooking the creek, while a pedestrian bridge provides pedestrian connectivity through this part of downtown and to the Midland Trace Trail.



*Public Plaza Connection*



*Enhancements to Grassy Branch*



*Promenade with Bridge Over Grassy Branch*

### PUBLIC PLAZA

This plaza plays a key role in connecting the community to Grassy Branch in a meaningful way. The plaza is a gateway to the trails and Grassy Branch recreational opportunities. It includes an overlook with educational and interpretive elements about the waterway.

The plaza should be completed in conjunction with adjacent development.

### TRAIL & RIPARIAN ENHANCEMENTS

Grassy Branch is a unique asset within the downtown. Enhancing this asset plays a key role in improving overall creek health and educating the community on ecological stewardship. Steps for these enhancements include:

- Removing invasive species
- Bank stabilization
- Reestablishing the riparian environment

These enhancements can be started immediately.

### PROMENADE & PEDESTRIAN BRIDGE

The promenade and pedestrian bridge play a key role in expanding connectivity within the downtown. The promenade should be constructed in conjunction with bank stabilization or development of the adjacent parcel.

The pedestrian bridge should be coordinated with overall riparian establishment to ensure minimal construction disturbance to the natural area.

## IMPLEMENTATION & PHASING | HORIZON PROJECTS

### HORIZON PROJECTS

While Catalyst and Key Projects are anticipated to be completed within early phases of the Redevelopment Plan, Horizon Projects are those that are on an extended timeline and may be completed as the general market conditions allow. The majority of these projects are residential in nature, and play a supporting role to the overall vision for downtown.

Important for all of these areas is the scale of any infill buildings. The Redevelopment Plan envisions higher density and taller buildings around SR 32, while gradually transitioning to a lower density and size of buildings as development occurs farther from the Downtown core. It is important to note that while size and density in these infill areas may not exactly match those of the abutting existing development, new development will still be complimentary in both of these aspects.

### HORIZON PROJECTS | GENERAL AREAS



## HORIZON PROJECTS | RESIDENTIAL AREAS

### DESIGN INTENT

As downtown grows, so too will its need for residents and housing. Providing a variety of housing typologies ensures residents living in downtown come from a broad range of backgrounds. This diversity among downtown residents promotes a downtown that serves a community rather than a single type of user group. Housing typologies that would be appropriate for downtown include dense single-family homes, townhomes, duplexes, triplexes, small scale apartment buildings, and large scale multi-family buildings.



*Townhomes and Dense Single-Family Homes*



*Infill housing of varying types*



*Townhomes and Other Attached Housing*

### WESTLEA NEIGHBORHOOD

Allowing accessory dwellings units in the neighborhood - or re-platting to smaller lots - would encourage natural growth over time for denser development. Lots could also be combined to create clusters of townhomes along Penn Street & Asa Bales Park.

#### Estimated Residential Units

Townhomes: 36

Single-Family Detached: 112

### INFILL RESIDENTIAL

With existing urban lots, north of Penn Street is primed for infill residential of varying types. Duplexes and small multi-family buildings would be contextually appropriate for this area.

#### Estimated Residential Units

135 units of various types

### RESIDENTIAL GROWTH

Envisioned as a natural area for residential growth, this site could support between 30-65 townhomes, depending on unit size and open space created.

#### Estimated Residential Units

30-65 townhomes.

# IMPLEMENTATION & PHASING | REDEVELOPMENT PLAN CHECK-IN, ASSESSMENT, & REVIEW

## PLAN IMPLEMENTATION

As part of the overall process for downtown redevelopment, periodic check-ins have been planned to ensure ongoing redevelopment efforts are aligned with the long term vision. This implementation plan is outlined below with reference to the adjacent schedule.

### Check-In

Conducted by Redevelopment Commission (RDC) and the Mayor.

- Occurs annually, at the beginning of the year.
- Commission reviews progress achieved during prior year, and aligns objectives with upcoming year.
- Provides opportunity for new any members to be read-in on Redevelopment Plan and have buy-in for year ahead.

### Assessment

Conducted by the Economic Development Department.

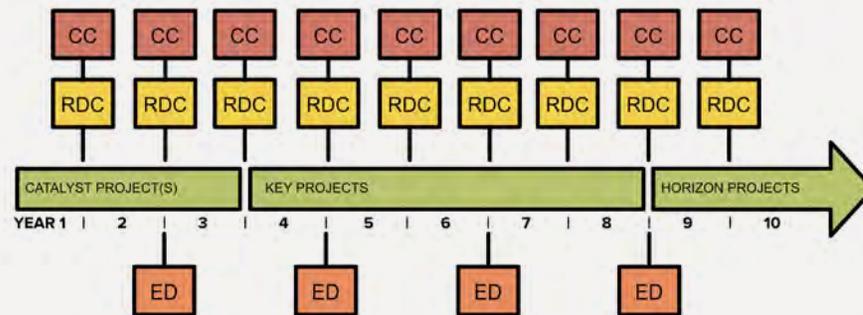
- Occurs bi-annually.
- Purpose is to identify major changes in market, community, or general planning and make necessary adjustments to Redevelopment Plan.

### Review

Conducted by City Council (CC)

- Occurs at major milestones. Such as new Catalyst Projects and Key Projects.
- Most likely to occur annually.

## PLAN IMPLEMENTATION SCHEDULE | MUNICIPAL ROLES



**CHECK-IN**  
(REDEVELOPMENT COMMISSION & MAYOR)

Occurs annually at beginning of year  
Redevelopment Commission reviews progress achieved during prior year, and aligns objectives with upcoming year.

**ASSESSMENT**  
(ECONOMIC DEVELOPMENT DEPARTMENT)

Occurs bi-annually  
Purpose is to identify major changes in market, community, or planning issues and make necessary adjustments to Redevelopment Plan.  
Example: COVID changed the way we work, so adaptations to offices and third spaces were necessary.

**REVIEW**  
(CITY COUNCIL)

Occurs at major milestones.  
Most likely to occur annually.

## SPACE FOR FUTURE UPDATES





# THREE

**CREATING THE VISION**  
DESIGN STANDARDS & GUIDELINES

# DEVELOPMENT STANDARDS & GUIDELINES | INTRODUCTION & USAGE

**DISCLAIMER: THESE STANDARDS AND GUIDELINES – AS OF OCTOBER 7, 2025 – REPRESENT A PRELIMINARY SET OF REGULATIONS FOR THE REDEVELOPMENT OF DOWNTOWN WESTFIELD. THESE REGULATIONS ARE SUBJECT TO CHANGE, AS ADDITIONAL REFINEMENT OF THESE STANDARDS AND GUIDELINES ARE FORTHCOMING THROUGH A COLLABORATIVE EFFORT WITH THE DEPARTMENT OF COMMUNITY DEVELOPMENT.**

## APPLICABILITY

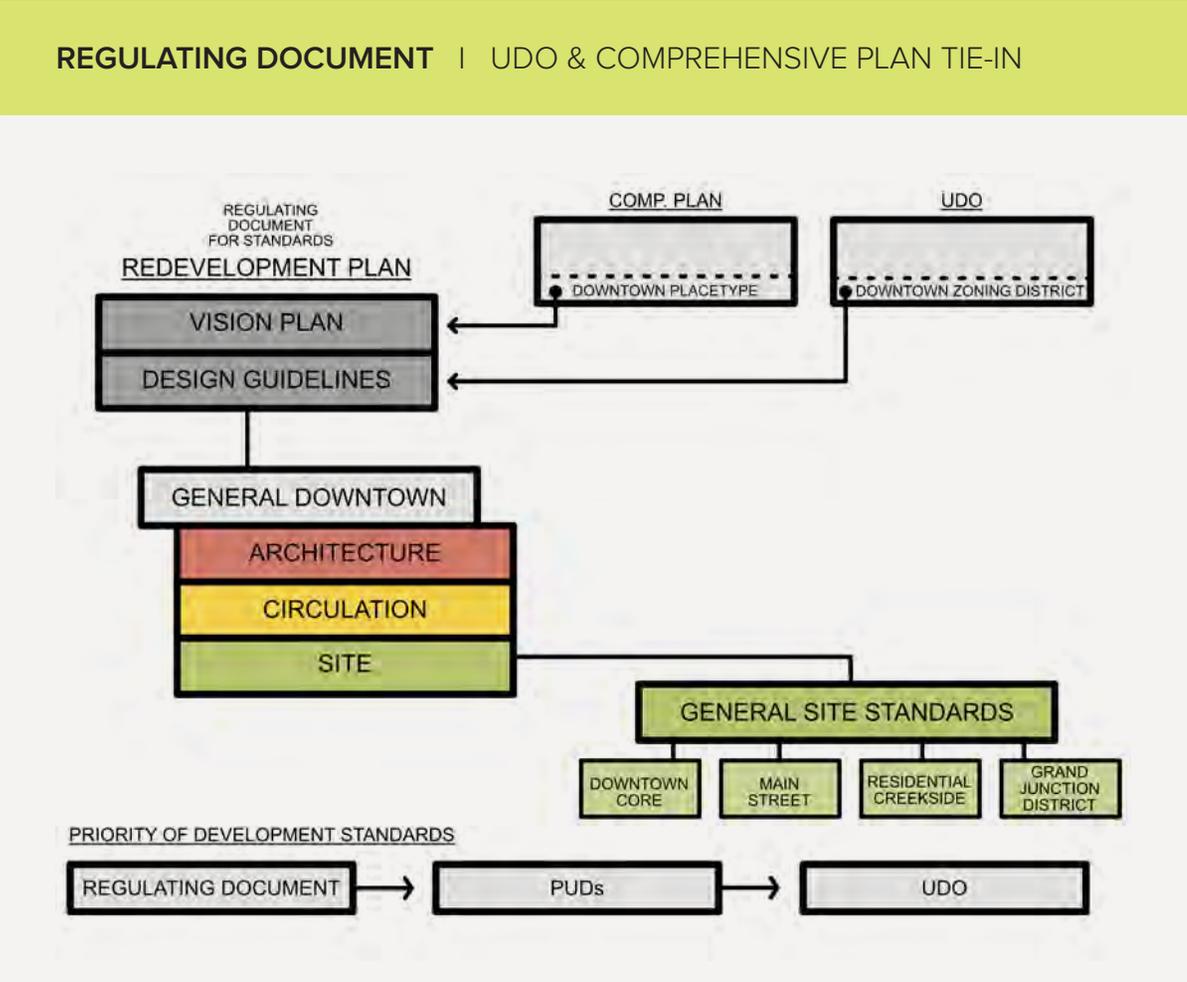
These design guidelines and standards are intended to complement Westfield’s Unified Development Ordinance (UDO).

The design standards and guidelines outlined within this Regulating Plan shall apply to the area designated Downtown District by the UDO. Design standards and guidelines not expressly indicated within this Regulating Plan shall default to standards of the corresponding underlying Zoning Districts listed below:

PLAN AREA	UDO ZONING DISTRICT
Downtown Core	LB-H (Local Business/Historical)
Main Street	LB-H (Local Business/Historical)
Legacy and Historic Core	SF-3 (Single-Family Medium Density)
Residential General/Infill	SF-5 (Single-Family High Density)
Residential Creekside	LB-H (Local Business/Historical)
Grand Junction District	LB (Local Business)
Downtown Adjacent Area	LB (Local Business)
Westfield Boulevard Area	MF2 (Multi-Family Medium Density)

Any existing Planned Unit Development (PUD) Zoning District located within the defined limits of the Downtown District shall be incorporated into the Downtown District with this Regulating Plan superseding in instances of conflict with standards of the UDO.

In instances where two or more provisions of this Regulating Plan conflict or are inconsistent, the provision that is most restrictive shall take precedence.



## REGULATING DOCUMENT

While the Redevelopment Plan is a stand-alone document, it plays a key role as the Regulating Document of the Downtown area for both the Comprehensive Plan and the Unified Development Ordinance. The Comprehensive Plan designates the Downtown area as part of the Downtown Placetype, however, the Plan does not provide guidelines for this area itself. Instead, it points to the Redevelopment Plan as the guiding document for Downtown Westfield.

In a similar manner, the UDO points towards this Redevelopment Plan document, dictating guidelines and standards for Downtown development.

## HOW TO USE THIS SECTION

1. Determine the Downtown Plan Area location.
2. Check for underlying PUD Ordinance.
3. Follow Plan Area standards and general Downtown district requirements.
4. Reference applicable Underlying Zoning District within the UDO.

## DEFINITIONS

Capitalized terms within this document shall have meanings ascribed to them as defined in Chapter 12 of the Westfield UDO. Definitions unique to this document are listed below, and shall supersede other known definitions. All other words shall have their meaning inferred by the context in this document or their ordinarily accepted definitions.

- Guideline(s) - Denotes subjective criteria indicating the spirit or intent of preferred design direction. Achieving guidelines is highly recommended.
- Standard(s) - Denotes objective criteria that provide specific directions or metrics. Compliance with development standards is required.
- Artist Studio - a space primarily used for the creation, production, display, and occasional sale of visual, performing, or fine and decorative arts. This includes but is not limited to painting, sculpture, ceramics, printmaking, photography, music, composition, craft work, textile art, or similar creative practice. This does not include businesses related beauty and personal care services such as nail salons, esthetic studios, tattooing, or body-piercing.
- Public Space - An area that is open and accessible to the public. It can encompass various locations including parks, streets, sidewalks, and plazas. Public spaces, which may be publicly owned, may also include privately owned areas that are accessible to the public, such as retail courtyards and seating areas. The key characteristic of public space is its accessibility and openness for public use and enjoyment.
- Setback - The horizontal distance between a structure and a lot/property line or right-of-way line.
- All setbacks shall be measured from the adjoining property line or right-of-way line. In no instance should a setback be measured from a curb, sidewalk, or other built infrastructure.

# DOWNTOWN WESTFIELD | PLAN AREAS



# DEVELOPMENT STANDARDS & GUIDELINES | PERMITTED USES

## PERMITTED USES

All use designations within the Underlying Zoning District shall apply except as otherwise modified below.

### Downtown Core (LB-H)

- Additionally Permitted Uses
  - Artist Studio
  - Bed and Breakfast
  - Hotel
  - Educational Institution, without Dormitory Accommodation (non tax-exempt)
- Additional Special Exceptions
  - Assembly Hall/Auditorium
  - Theater, indoor
  - Engineering, Research, Testing Laboratories

### Legacy and Historic Core (SF-3)

- Additionally Permitted Uses
  - Art Studio
  - Dwelling, duplex
  - Townhouse
- Additional Special Exceptions
  - Bed and Breakfast
  - Office
  - Office, Medical
  - Club, Civic

### Residential General/Infill Area (SF-5)

- Additionally Permitted Uses
  - Dwelling, Efficiency Unit
  - Dwelling, Quadriplex
  - Dwelling, Triplex
  - Multi-Family Small

- Additional Special Exception
  - Dwelling, Single-Family (Maximum Ground Floor SF: 2,800 sf)
  - Bed and Breakfast

### Residential Creekside (LB-H)

- Additionally Permitted Uses
  - Dwelling, Efficiency Unit
  - Dwelling, Quadriplex
  - Dwelling, Triplex

### Grand Junction District (LB)

- Additionally Permitted Uses
  - Art Studio
- Additional Prohibited Uses
  - Nursing Home
  - Auction Rooms
  - Childcare/Daycare Centers
  - Garden and Lawn Center
  - Gasoline Service Station
  - Health, Fitness, and Exercise center
  - Mortuaries
  - Office, Medical
  - School, Professional Trade or Business
  - Veterinary Hospital, Large Animal
  - Educational Institution, without Dormitory Accommodations
  - Hospital
  - Religious Institution
  - Composting Facility, commercial
  - Data Processing/Call Center
  - Material Recycling Facility (MRF)
  - Nursery

### Downtown Adjacent Area (LB)

- Additionally Permitted Uses
  - Art Studio
  - Dwelling, Single-Family
  - Dwelling, Duplex
  - Dwelling, Quadriplex
  - Dwelling, Triplex
  - Bed and Breakfast
  - Health, Fitness, and Exercise Center
  - Hotel
  - Office
  - Office, Medical
  - Restaurant, Sit Down
  - Restaurant, Specialty
  - Restaurant, Takeout and Deli-style
  - Retail, Low Intensity
  - School, Fine Arts or Commercial
  - Club, Civic (non tax-exempt)
  - Club, Private (non tax-exempt)
  - Park, Public
- Additional Special Exception
  - Tavern
  - Assisted Living Facility
  - Hospice
  - Nursing Home
  - Childcare/Daycare
  - Religious Institution

### Main Street (LB-H)

- Additionally Permitted Uses
  - Art Studio

### Westfield Boulevard District

Uses in this area are defined by the Grand Millennium Center PUD.



# ARCHITECTURE

## STANDARDS | GUIDELINES

# A

---

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

The standards within this regulating document shall supersede Chapter 6.3 Architectural Standards of the UDO. Photos within the following section have been specifically chosen to show intent of associated guideline or standard.

## GENERAL MASSING

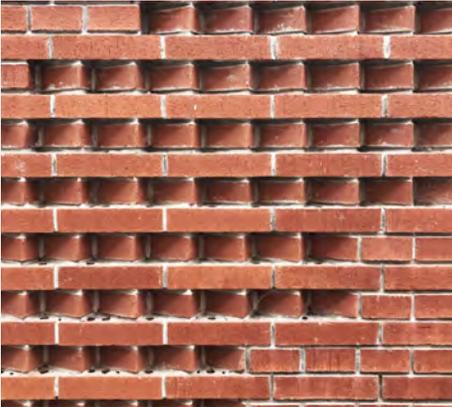
### Guidelines

- Building massing should be designed with changes and variations in form that reflect the uses of the building.
  - Articulation may occur in the vertical and/or horizontal plane as appropriate for the function of the building.
- Massing should be visually compatible with the general scale and character of adjacent development.
- Building volume should be utilized to frame and define the public realm including streetscapes, courtyards, and plazas.
- The ground floor facade should engage the street with active uses, architectural detailing, transparent glass, awnings/canopies, recessed entries, and appropriate wall articulation.

### Standards

- A. Building Frontage - A minimum of 100% frontage onto a public street is required for all buildings, except residential uses requiring a setback, unless the portion of a building not fronting is used for a public plaza.
- B. Buildings shall include a clear and architecturally cohesive ground floor with a visible change in the facade. The ground floor must include at least one of the following:
  - i. A change in material;
  - ii. Texture;
  - iii. Pattern;
  - iv. Ornamentation; or
  - v. A change in depth no less than 6 inches.
- C. All street-facing building facades shall include a rhythm of repeating architectural elements along the facade's length. Single-family detached buildings are exempt from this standard.
- D. Articulation - Shall be provided for any elevation that faces a public right-of-way.
  - i. Must be at least 1/3 of the elevation length.
  - ii. Must be at least 2' in depth.
- E. All buildings, except single-family detached structures, shall not include long, monotonous, and/or blank facades. If these cannot be avoided due to internal programming, then they must be enhanced with one or more of the following features:
  - i. Vegetation
    - a. May be columnar or ornamental tree varieties that reach a minimum mature height of 20'.
    - b. Trellis(es) that cover 30% of the blank wall with vines or other creeping plant species that will grow to sufficient density to cover the wall.
  - ii. Artwork
    - a. Murals - Must cover 30% of the wall area. Refer to the Westfield Art Master Plan for additional mural requirements and incentive opportunities.
    - b. Mounted sculptures, or other artwork approved by the Planning Department. Refer to Westfield Arts Master Plan for detailed standards.
  - iii. Architectural detailing - including textured masonry techniques, must cover at least 40% of the wall area.

## DETAILING | STANDARD



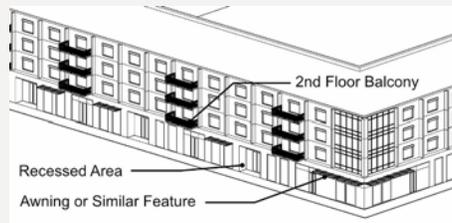
Textured Masonry Examples

## PROTECTION FROM ELEMENTS

### Standards

- A. Balconies - The uppermost balcony for an attached residential unit shall be provided with a protective element to provide shade and shelter from rain, snow, and sun.
- B. Building Facades - All south, west, and east facing facades, within 5' of a public sidewalk or pedestrian trail, shall provide sun and rain protection in the form of an awning or other architectural element that provides shelter.
  - i. Awnings or other architectural elements shall be required along a Building Facade fronting a Right-of-Way for a minimum of 50% of the Building Facade length, and project at least 5' from the Building facade.
  - ii. A recessed area along the Building Facade may contribute towards meeting this requirement. Recessed areas shall be a minimum of 5' in depth.

## PROTECTION | STANDARD



## BALCONIES | STANDARD



Element must be no higher than 9 feet above balcony floor.

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## BUILDING ENTRIES

### Guidelines

- Public entries shall be easily identifiable and visually enhanced with color, texture, material, and/or form. Sculptural seating may be utilized to enhance building entries.
- Entries with direct access to a residential unit should be separated from the ground plane by a vertical elevation change, screening, and/or landscaping. Entries shall include a distinct porch, patio, or other clearly private space to distinguish from the public way.

### Standards

- A. Address numbers - All building entries shall have clearly marked address numbers.
  - i. Letters shall be sized at least 1" in height per 10 linear feet of distance from the edge of pavement on adjacent streets.
- B. Corner entry - If used, is required within 20' of a building corner.
- C. Entries must be featured in one or more of the following ways:
  - i. A change in plane by a minimum of 2'.
  - ii. Recessed doorway.
  - iii. Overhang, canopy, or other architectural feature with a distinct form that is larger than other similar features on the same facade.
  - iv. Unique windows above the doorway, that are not featured on other parts of the building.
  - v. Glass feature windows on both sides of the

- vi. entry doorway.
- vi. Ornamental light fixture over or flanking the entry door.
- vii. Architectural ornamentation over or flanking the entry door.
- viii. Floor to ceiling entry doors.
- ix. Planters with landscape framing the entry.
- x. Sculptural seating at the entry.
- xi. Stone pavers, bricks, other material, and/or texture change in ground plane materials.
- xii. Low landscape beds - must be 2' or lower in height.
- xiii. Railings or other physical separation.
- xiv. Landscape treatments indicating the separation between public and private space.
- xv. Pilasters or columns supporting and/or framing the entrance.
- xvi. An elevated or depressed entrance from the adjacent sidewalk through the use of stairs or ramps.

## ENTRIES | STANDARD



## BUILDING MATERIALS

### Guidelines

- Residential garage doors should be consistent with the color and/or material of the primary or accent materials.

### Standards

*Primary Material - a material or texture/color used on a minimum of 40% of a building facade.*

*Accent Material - a material or texture/color used on a maximum of 20% of a building facade.*

- Building facades are limited to a maximum of 3 primary materials and colors.
- An accent material/color may be used for no more than 20% of a facade.
- Prohibited Materials - The following materials are expressly prohibited:
  - Vinyl siding:
    - Vinyl windows are permitted if color is consistent with palette of other exterior materials.
  - Stucco (except 3-coat rainscreen systems)
  - E.I.F.S.
  - Concrete block without color or decorative finish.
  - Concrete without architectural finish.
- Material Returns - Building materials that extend to a corner should make a proper return at least two times the depth of the material.

## MATERIALS | STANDARD



**ACCEPTABLE** - Building uses two primary materials in a logical manner, and uses an accent color to highlight the ground floor.



**NOT ACCEPTABLE** - Building uses 3+ materials and colors in a haphazard manner, creating visual clutter and confusion.

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## WINDOWS

### Guidelines

- Street-level windows for residential uses on the ground floor should incorporate some type of partial window frosting or curtain/blind device that provides residents with privacy from the street while still permitting natural light to enter the unit.

### Standards

- A. Windows shall be provided along all Building Facades facing a Right-of-Way, plaza, or other public space.
  - B. Floors other than the ground floor shall include a minimum of 25% window area calculated per story.
  - C. Ground floor commercial, mixed-use, institutional, civic buildings, and retail tenant spaces shall have 50% ground floor window glazing as measured 0-10 feet above adjacent grade.
    - i. Exceptions, as determined by the Director, may be made where appropriate based on the ground floor program.
  - D. Ground level residential units shall provide window area for at least 25% of the facade.
- E. Window Shading - At least 30% of windows within 45 degrees of facing south or west should be shaded by an awning, eave, blade, or other architectural feature.
    - i. The shading device must project at least 3 feet unless otherwise indicated through a solar design analysis.
    - ii. Shading must provide 45-55% shading at the equinox, 90-100% shading at the summer solstice, and 0-10% shading at the winter solstice.
  - F. Windows on the ground floor shall include a bulkhead, lintel, or other appropriate design feature to cap the windows and transition to the upper building facade.

## WINDOWS | GUIDELINE



## MECHANICAL SCREENING

### Standards

- A. Mechanical, electrical, utility, and communication equipment, regardless of location, shall be fully screened from the adjacent street level and public spaces. Screening may consist of opaque enclosures, enlarged parapets, prefabricated screens, or other means that are consistent with the development's character.
- B. Wall-mounted mechanical equipment and thru-wall units shall be designed to minimize visibility either through screening or integration into the building's overall architectural design.
- C. Painted or adhesive-applied artwork is an acceptable alternative to screening equipment. See Westfield Arts Master Plan for further information.

## SCREENING | STANDARD



## BUILDING LOGISTICS

### Standards

- A. Solid waste and recycling collection area shall be interior to the building or behind constructed walls and opaque access doors consistent with the exterior architecture of the building.
- B. Loading docks shall be fully obscured from streets, plazas, and public ways.
- C. Chain-link fencing is not permitted.

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## RESIDENTIAL BUILDING TYPES

As Downtown Westfield develops, providing a variety of housing types is essential for building a resilient, vibrant, and sustaining Downtown community. A mix of housing options — ranging from single-family homes to apartments — ensures people of different ages, incomes, and lifestyles can find appropriate places to live and call home. Offering multiple housing choices also supports economic vitality, as a balanced supply can help to meet housing demand, stabilizes costs, and attract new residents.

By weaving together varied housing forms the community can create complete neighborhoods that reflect the needs of their people today while preparing for future generations.

### Key Benefits:

- Affordability and housing choice.
- Encourages multi-generational communities.
- Strengthens long-term economic and social resilience of Downtown by supplying a broad range of residents to invest in local businesses.

## RESIDENTIAL BUILDING TYPES | PERMITTED PLAN AREAS

	Single-Family Detached	Townhome	Duplex Triplex	Multi-Family Small	Multi-Family Large	Mixed Use
Downtown Core		✓		✓	✓	✓
Main Street						
Historic & Legacy Area	✓	✓	✓	✓		
Residential - Infill	✓	✓	✓	✓	✓	
Residential - Creekside		✓	✓	✓	✓	✓
Grand Junction District		✓	✓	✓	✓	✓
Downtown Adjacent	✓	✓	✓	✓	✓	✓
Westfield Boulevard Area					✓	✓

## Single-Family Detached

### Standards

- A. Target Density: 4-5 du/ac
- B. Setbacks:
  - i. Front - 8' minimum.
  - ii. Side - 3' minimum.
  - iii. Rear - 5' minimum.
- C. Garage Entry:
  - i. Must be located to the rear of the lot with alley access or setback 10' from the front facade of the primary structure.

### DESIGN | ACCEPTABLE



*Meet Standards for Design and Character*

### DESIGN | NOT ACCEPTABLE



*Not consistent with existing character /  
Garages too prominent*

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## Townhouses

Typically a row of 3 to 8 single-family dwellings with a common wall between each unit. May include attached or detached parking.

## Guidelines

- Townhomes should be of simple and clear form, void of unnecessary articulation. Historic forms are encouraged where appropriate.
- Individual units should be articulated and the building should appear as a cohesive whole.
- Patio and balcony spaces are highly encouraged.

## Standards

- A. Target Density: 20-30 du/ac.
- B. Maximum Height - 3.5 stories, or 40' (except at Creekside Residential Plan Area).
- C. Setbacks:
  - i. Front - 8' minimum, porches or stoops may project up to 4' into the front setback.
  - ii. Side - 5' minimum.
  - iii. Rear - 20' minimum.
    - i. Townhomes along Asa Bales Park may reduce setback to 8' at the rear property line if the structure is oriented towards the park.
- D. Building Massing:
  - i. The Front Building Facade must articulate by at least 2' every 25' along the front facade.
- E. Garages:
  - i. Front loaded garages are permitted if garage door is consistent with the primary materials.

## DESIGN | ACCEPTABLE



*Cohesive individual units, uniform materials*

## DESIGN | NOT ACCEPTABLE



*Not cohesive, mixed materials*

### Duplexes & Triplexes

Duplexes and triplexes are single buildings with 2-3 units. These units may be side-by-side with a common wall between or split vertically with one unit on the ground floor and one or more units on upper floors.

### Guidelines

- Form and massing should present as a cohesive whole.
- Roofs should be simple in form and function, multiple overlapping roof lines are discouraged.
- Covered front porches are highly encouraged.
- Garage doors should not present as the 'front door' of the units.

### Standards

- A. Target Density: 15-20 du/ac.
- B. Maximum Height - 3 stories
- C. Setbacks:
  - i. Front - 8' minimum.
  - ii. Side - 5' minimum.
  - iii. Rear - 20' minimum.
- D. Garages:
  - i. Front-loaded garage doors must be in-line or behind the front building facade.
  - ii. Must match the color or tone of the primary building material, or be of a natural wood tone.

### DESIGN | ACCEPTABLE



*Cohesive with appropriate scale & character*

### DESIGN | NOT ACCEPTABLE



*Inappropriate character for Downtown*

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## Multi-Family Small (4-20 units)

A building made up of a group of attached units, often in several buildings in one development, typically for lease. These may range from a two story building with 4 units each to a series of 20 units spread across 2+ floors.

### Guidelines

- Buildings should be contextual with its surroundings, in form, style, and material use.
- Slot homes should generally not be developed, and other site layouts should be explored.
- Courtyards and other common areas are encouraged.
- Balconies and patios are encouraged.

### Standards

- A. Target Density: 30-40 du/ac.
- B. Maximum number of Units Per Building - 20.
- C. Maximum Height - 3 stories.
- D. Setbacks:
  - i. Front - 0' minimum.
  - ii. Side - 5' minimum.
  - iii. Rear - 20' minimum.
- E. Parking:
  - i. Must be located behind the building or along the side yard. Where visible from public way, screening via landscaping or fencing required. Fencing should be of quality materials and consistent with primary facades.

## DESIGN | ACCEPTABLE



*Cohesive individual units, uniform materials*

## DESIGN | NOT ACCEPTABLE



*Non-cohesive massing, slot homes*

### Multi-Family Large (20+ units)

A multi-story building made up of 20+ multi-family units. Units may be wrapped around a central parking garage.

#### Guidelines

- Buildings should be simplistic in massing and generally avoid too much facade articulation.
- A distinct base is not necessary for these building types where there is no ground floor retail use. Where there is ground floor retail uses, that level should be articulated as a unique architectural expression with complimentary materials and design.
- Ground floor residential units should be provided with window treatments that provide visual privacy from the street but still let in unfiltered natural light.
- Centralized amenity spaces are encouraged.

#### Standards

- A. Target Density: 50-60 du/ac.
- B. Setbacks:
  - i. Front - 0' minimum.
  - ii. Side - 8' minimum.
  - iii. Rear - 0' minimum.
- C. Ground floor minimum height - 15 feet.
  - i. Measured from the finished floor to the bottom of the primary structure, including supporting beams.
- D. Ground floor minimum depth - 25'.
  - i. Measured from the street facing facade.
- E. Balconies - any provided balcony, projecting or recessed, must be a minimum of 5' in depth.

### DESIGN | ACCEPTABLE



*Appropriate scale and material use*

### DESIGN | NOT ACCEPTABLE



*Visually busy form material use*

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## MIXED USE

A building with a mixture of commercial and residential uses. Typically, retail or office spaces are located on the ground floor with apartments in the stories above. Some mixed use buildings may have a top floor commercial space like a bar associated with a restaurant on the ground floor.

### Guidelines

- The ground floor should be treated with a distinct base to highlight the mixed-use aspect of the building.
- Generally, building facades should reflect the primary functions and uses with the an articulated base as noted above, and a residential layer above. An artificial “base, middle, top” facade treatment using stories is to be avoided.
- Rooftop amenities spaces are encouraged.
- Window locations should relate to internal spaces.

### Standards

- A. Maximum Height - 7 stories.
- B. Setbacks:
  - i. Front - 0' minimum.
  - ii. Side - 0' minimum.
  - iii. Rear - 0' minimum.
- C. Parking:
  - A. Must be located behind the building or integrated within. Parking is required to be fully screened from public view.

## DESIGN | ACCEPTABLE



*Simple cohesive architecture.*

## DESIGN | NOT ACCEPTABLE



*Too many materials and incoherent volumes*

## COMMERCIAL (RETAIL & OFFICE)

Typically a structure designed for business activity, featuring flexible ground-floor retail or service spaces, upper-level offices, and meeting spaces.

*Note: Refer to Plan Areas section of this document for guidance on requirements for Main Street Plan Area architecture.*

### Guidelines

- Brick, stone, and other high-quality facade materials are highly encouraged.
- Historic character and forms should be emphasized.

### Standards

- A. Maximum Height - 7 stories.
- B. Setbacks:
  - i. Front - 0' minimum.
  - ii. Side - 0' minimum.
  - iii. Rear - 0' minimum.
- C. Parking:
  - A. Must be located behind the building or along the side yard. Parking is required to be fully screened from public view.

### DESIGN | ACCEPTABLE



*Cohesive and logical use of simple materials*

### DESIGN | NOT ACCEPTABLE



*Inconsistent or inappropriate material use.*

# ARCHITECTURAL STANDARDS & GUIDELINES | ALL DEVELOPMENT

## PARKING GARAGE

A multi-level structure designed to centralize parking spaces.

### Guidelines

- Garages should be easily accessible to both vehicles and pedestrians, promoting a “park once and walk” development pattern. Secondary functions, such as pick-up and drop-off, waste management, and storage, may also be suitable on the ground floor of a garage.
- Signage and/or architectural expression should make the location and access to the garage clear to vehicle traffic adjacent roads.

### Standards

- A. All parking garages facing a public right-of-way shall have decorative art, screening, or vegetation to screen and/or visually enhance the structure’s facade.
- B. Garage facades must utilize soft screening material, such as vegetation, when directly across the street from residential uses. Facades across from mixed-use residential are exempt.
- C. Openings with no infill, such as those between structural elements, shall either be covered in screening or cladding, or remain open with appropriate guards and/or railings as required by code.
- D. A maximum of 10% of exposed garage face may remain unscreened.

### DESIGN | ACCEPTABLE



*Creates cohesive screening*

### DESIGN | NOT ACCEPTABLE



*Does not screen parking decks*

PAGE INTENTIONALLY LEFT BLANK



# CIRCULATION **B**

## STANDARDS | GUIDELINES

---

# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

## STREET TYPES

Streets within the Downtown can play a unique role in creating a successful built environment. The following street types are coordinated with those in the Comp. Plan, and are considered guidelines for redevelopment.

### Regional Boulevard

Intended for high-capacity thoroughfares, ideal for commuting traffic and main corridors through large areas.

- State Road 32

### Crosstown Avenue

Important corridors that balance traffic movement with bicycle and pedestrian infrastructure.

- Westfield Boulevard

### Mixed-Use/Low Density Commercial Connector

Designed for mixed-use areas, with a strong emphasis on multi-modal transportation.

- Union Street

### Neighborhood Avenue

Primarily located in neighborhoods and other areas that have limited through traffic.

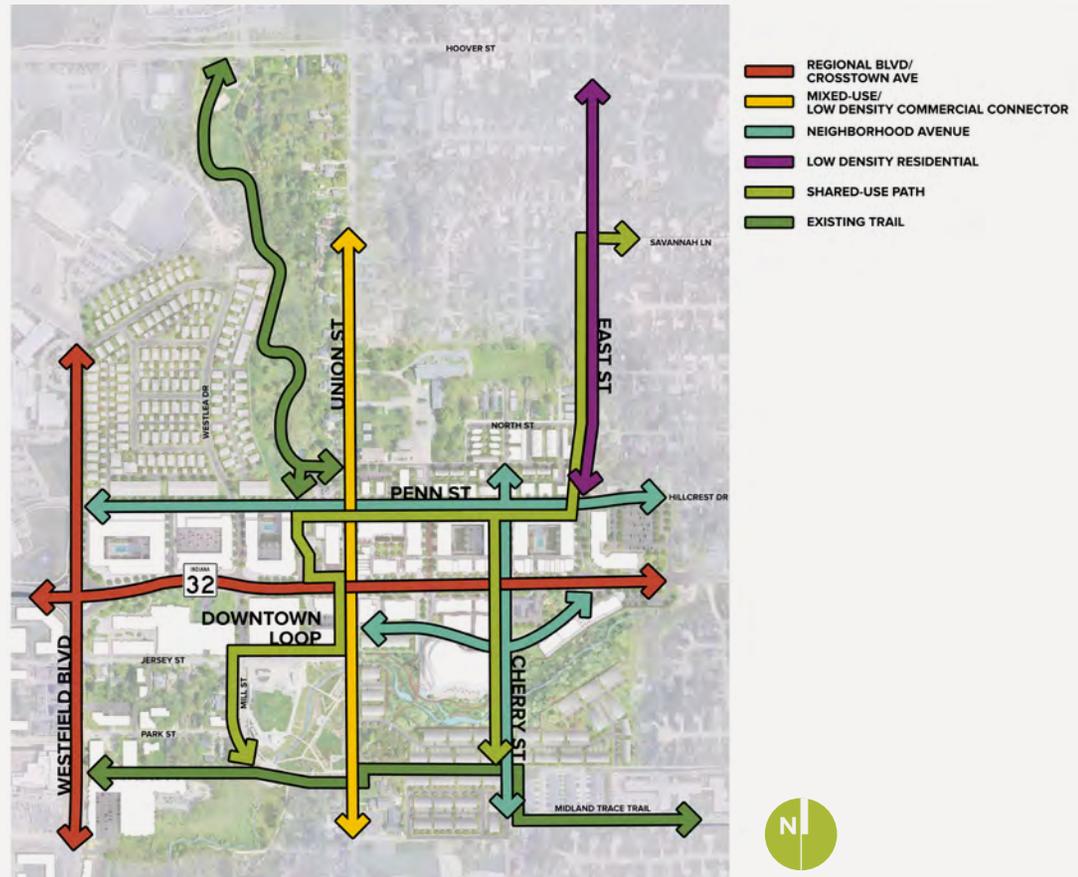
- Penn Street
- Jersey Street
- Cherry Street

### Low Density Residential

Typically located in areas of low-density residential. Likely has minimal infrastructure.

- East Street

## CIRCULATION | BY STREET TYPE



### **Regional Boulevard - Design Criteria**

High Capacity streets designed for regional connectivity and traffic movement across broad areas. Serve as main corridors for commuting and freight movement, with some accommodations for multi-modal users. They balance vehicle throughput with safety and access.

#### **A. Typical Characteristics**

- i. Multiple travel lanes each direction with dedicated turn lanes. May include medians.
- ii. Limited, if any, street parking.
- iii. Pavement width typically 48+ feet.
- iv. Designed speed is generally 40-55 mph.
- v. Functional Class – Arterial.
- vi. Level of Service - D or better preferred.

#### **B. Standard Design**

- i. Travel Lane – 11' to 12' wide.
- ii. Parking Lane – 9' wide.
- iii. Turning Lane – 10' minimum.
- iv. Sidewalk – 8' - 10' clear path.
- v. Buffer – 2' min from back of curb to clear path of sidewalk.
- vi. Median – 6' width recommended. Should include a refuge area for pedestrians at all crossings.
- vii. Landscape Zone – Minimum 5' of width
- viii. Pedestrian Activity Zone – 6' minimum on each side of the street.
- ix. Vehicle Access – limited to public streets and alleys. Curb cuts to private drives should not be permitted.

### **Crosstown Avenue - Design Criteria**

Important for connecting neighborhoods to commercial centers and arterial streets. Designed to balance moderate vehicle flow with pedestrian, bicycle, and transit accessibility.

#### **A. Typical Characteristics**

- i. May have single-lane in each direction, some dedicated turn lanes at intersections.
- ii. Limited, if any, street parking
- iii. Designed speed is generally 35-45 mph
- iv. Functional Class – Minor Arterial/Collector
- v. Level of Service - C-D typical

#### **B. Standard Design**

- i. Travel Lane – 10' to 12' wide
- ii. Parking Lane – 9' wide
- iii. Turn Lane – 10' wide
- iv. Bike Lane/Shared-Use Path – Min. 8' width for bi-directional travel, with 2' clear on both sides.
- v. Buffer – 2' minimum from back of curb to sidewalk or bike lane/multi-use path clear path.
- vi. Sidewalk – 6' - 8' clear path

### **Mixed-Use/Low Density Commercial Connector - Design Criteria**

Street type designed for mixed-use areas combining residential, commercial, and office uses. Balances vehicular flow with a strong emphasis on pedestrian and bicycle access.

#### **A. Typical Characteristics**

- i. Single-lane in each direction, may have a center turn lane.
- ii. Street parking is available, may be limited to one side in some areas.
- iii. Some dedicated turn lanes at high traffic intersections.
- iv. Designed speed is generally 30-40 mph.
- v. Functional Class – Minor Arterial/Collector.
- vi. Level of Service - C-D typical.

#### **B. Standard Design**

- i. Travel Lane – 10' to 11' wide.
- ii. Parking Lane – 8' to 9' wide.
- iii. Bike Lane/Shared-use Path – 8' minimum width for bi-directional travel with 2' clear on each side.
- iv. Buffer – 2' minimum when parking lane is adjacent to bike lane or shared-use path.
- v. Sidewalk – Minimum 6' wide.

# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

## SR 32 Modifications

To ensure SR 32 feels like a cohesive part of the Downtown - not simply a commuter thoroughfare - a series of traffic calming recommendations has been assembled to promote slower and more aware driving in the Downtown area. This ensures road noise is reduced, mitigates conflicts with pedestrians due to speed, and overall promotes a Downtown that is appropriately balanced for both driver and pedestrian.

## SR 32 Recommendations

Recommendations for traffic calming with potential implementation options.

- A. Narrow lanes to 10' to encourage slower driving.
  - i. Modify and relocate striping.
- B. Reduce curb radii.
  - i. Paint, flexible delineators
- C. Bumpouts at Pedestrian Crossings.
  - i. Temp - paint, flexible deliniators.
  - ii. Perm - extended curb.
- D. Establish street tree canopy.
  - i. Regular trimming, no topping.
- E. Enhanced pedestrian crossings.
  - i. Unique crosswalk patterns.
  - ii. Raised crosswalks.
- F. Raised intersection.
  - i. At Union Street, primary pedestrian crossing.
- G. Reduce curb radii.
  - i. Paint and flexible deliniators.

## SR 32 | SPECIFIC DESIGN

- Two 11'-12' travel lanes in each direction
- Combined landscape buffer and sidewalk, typically 14' but 8' at the narrowest portions.
- Median that varies from 2' to 12' wide.
- Dedicated left turn lanes at Westlea Drive and Union Street
- Right-in, right-out access at Mill Street, North Walnut Street, North Cherry Street
- Right-in only at South Cherry Street
- Right-out only at South Walnut Street
- Additional setback requirement up to 15' for buildings fronting onto SR 32 from Union Street to East Street.



### Neighborhood Avenue - Design Criteria

Primary street within residential and mixed-use neighborhoods. Support local travel, walking, biking, and school or recreational access.

#### A. Typical Characteristics

- i. Single-lane in each direction, traffic calming may limit to a single bi-directional shared lane at some points.
- ii. Street parking available on both sides of the street.
- iii. Golf cart parking is designated at corner parallel spaces.
- iv. Trail or shared-use path is highlighted.
- v. Unlikely to have lane markings.
- vi. Designed speed is 20-35 mph.
- vii. Functional Class – Collector/Local.
- viii. Level of Service - B-C typical.

#### B. Standard Design

- i. Travel Lane – 10' wide.
- ii. Parking Lane – 8' wide.
- iii. Sidewalk – minimum of 6' wide.
- iv. Vehicle Access – curb cuts permitted to private lots.

## PENN STREET | SPECIFIC DESIGN

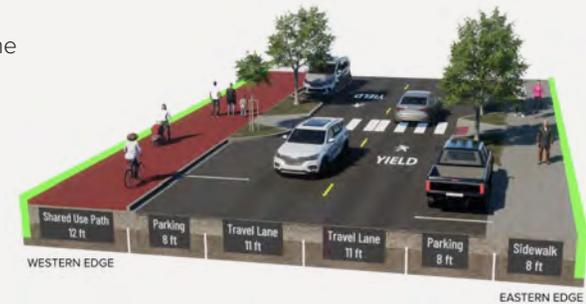
- Two 11' wide travels lanes, one in each direction.
- 8' wide parallel parking on the north and south sides of the street.
- Minimum 4' wide tree lawn on the north and south sides of the street.
- A minimum 6' wide sidewalk on the north side of the street.
- Shared-use path located on south side of street - minimum 12' wide.
- Golf carts permitted on street.
- Cart parking available on-street.
- Bumpouts at pedestrian crossings.



# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

## NORTH CHERRY STREET | SPECIFIC DESIGN

- A 12' wide (minimum) shared-use path is to be established on the west side of Cherry Street, with the purpose of connecting SR 32 to Penn Street.
- Two travel lanes - one in each direction - with a typical width of 11' each.
- Parallel street parking is provided on each side of the street.
- Crossing opportunities for pedestrians are enhanced with bumpouts.
- Some areas may forego street parking spaces for additional landscape area or cafe seating.



## SOUTH CHERRY STREET | SPECIFIC DESIGN

- Cherry Street between SR 32 and Jersey Street is one-way south, with a right turn only from SR 32. A northbound one-way at Walnut Street between SR 32 and Jersey Street offers a right turn only back onto SR 32 to create a balanced loop for the block.
- A 12' wide (minimum) shared use path on the west side of Cherry Street connects the Midland Trace Trail to SR 32.
- A lane of parallel parking is located on the west side of the road, providing a buffer between traffic and the shared use path. Keeping parking on the right side maintains familiarity for drivers parallel parking.
- Where space is available, a 5' tree buffer is recommended between the shared path and roadway.



### Low Density Residential - Design Criteria

Low density residential streets connecting rural homes and farms, accommodating local access with minimal infrastructure.

#### A. Typical Characteristics

- i. Single-lane in each direction, may have passing blister at some intersections.
- ii. Street parking unlikely.
- iii. May or may not have curbs.
- iv. Designed speed is 20-35 mph.
- v. Functional Class – Minor Collector/Local.
- vi. Level of Service - C or better.

#### B. Standard Design

- i. Travel Lane – 11'-12' wide.
- ii. Sidewalk – minimum of 6' wide.
- iii. Vehicle Access – curb cuts permitted to private lots.

## EAST STREET | SPECIFIC DESIGN

- Shared-Use Path is to be established on the west side of East Street, with the purpose of connecting Downtown to Freedom Park via Savannah Lane.
  - Minimum 12' wide clear path with 12-inch stone shoulder on both sides.
- Where space is available, a 5' tree buffer is recommended between the shared path and roadway.
- Maintain existing 10' - 11' travel lanes in each direction.
- Critical Intersections
  - Penn Street and East Street
  - East Street and Savannah Lane



# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

## Shared-Use Path/Trail Enhancement - Design Criteria

### A. Typical Characteristics

- i. Shared-use path for use by a mix of pedestrians and cyclists. Golf carts may be permitted in some areas where indicated.
- ii. Typical width is 8' to 16' wide, width may vary depending on conditions.
- iii. Cyclists and golf cart users should yield to pedestrians at all times.
- iv. Signage and wayfinding prevalent to direct and organize users.

### B. Standard Design

- i. Minimum 8' clear path with a 2' buffer clear of obstructions.
- ii. Pavement Markings:
  - a. Centerline striping is discouraged unless used at corners with limited sight distances or approaches to street intersections.
  - b. Any centerline striping shall be marked with pavers or other durable material. Paint, thermally applied graphics, and similar markings are prohibited.
- iii. Tactile Pavers:
  - a. Required along all pedestrian trails and through all public spaces.
  - b. Directional tactile pavers.
    01. Provide along all paths and trails, so that complete loops while using the pavers is possible.

02. Provide directional pavers from the main trail path to points of interest, intersections, and building entries.
03. Directional pavers should be placed to provide an unobstructed path through a public plaza.
04. Pavers may be color-matched to adjacent materials.

#### iv. Truncated Warning Blocks:

- a. Must be provided at all street crossings.
- b. Shall be provided at potential conflict points between pedestrians and other trail users, such as at the edge of golf cart parking that is provided adjacent to a trail.

#### v. Separation from Parking Lanes:

- a. A minimum 4' buffer zone shall be provided when shared-use path is adjacent to parallel street parking. This is to prevent cars doors from being opened into the path of path users. Buffer zone should be visually and/or textural distinct from the clear path of the shared-use path.
- b. Wheelstops are required for 90 degree or angled parking spaces adjacent to a shared-use path. Bollards are an appropriate substitute for wheelstops if provided at a ratio of 1 bollard per parking space.

#### vi. Vehicle Access:

- a. Collapsible bollards are to be placed at the intersection of a shared-use path and roadway. Bollards should collapse in a manner that allows emergency and service vehicles to access the shared-use path.

#### vii. Distance Markers:

- a. Pavement markings indicating distance travel along the shared-use path shall appear in quarter-mile increments.

#### viii. Bollards:

- a. A crash-rated bollard shall be placed at the intersection of all shared-use trails and streets to prevent vehicle intrusion onto the trail.
- b. Removable or retractable bollards are recommended for areas where vehicle access is necessary from time to time.
- c. The color, shape, and other characteristics of bollards shall contrast with their environment to prevent collisions by pedestrians, cyclists, golf cart users, and vehicles.



## Alley

### A. Typical Characteristics

- i. Two way traffic.
- ii. Typically have no dedicated sidewalks.
- iii. Narrower than typical roadway.
- iv. All users share the same space.
- v. Typically only used by service vehicles, or residents accessing private garages.
- vi. Maximum speed is typically 10 mph.

### B. Standard Design

- i. Minimum width:
  - a. 12' when adjacent to residential uses.
  - b. 16' when adjacent to 1 more commercial uses.
- ii. Sidewalk - not required.
- iii. Landscaping - not required.

# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

## GOLF CARTS

### Standards

- A. Parking:
- Should be provided on-street when appropriate. Ideally this parking is located in the parallel parking space closest to a street corner and striped for at least 3 carts to park perpendicular to the curb. Placing carts in this corner position allows better visibility across traffic as carts are typically open air and less visually obstructive than a parked vehicle.
- B. Travel:
- Permitted on all streets with a posted speed limit of 20 mph or less.
  - Permitted on Midland Trace Trail.
  - Prohibited on Downtown Loop Trail.
  - Prohibited in Asa Bales Park.
- C. Signage and Pavement Markings:
- Pavement markings required on all public streets where golf carts are permitted, at beginning of routes, and at intersections where direction of travel changes.
  - Signage required at beginning of route, at intersections where direction of travel changes, and along routes every 1/4<sup>th</sup> mile.

### GOLF CARTS | PARKING



*Carts Parking 3 to 1 Vehicle Space*

### GOLF CARTS | SIGNAGE



*Cart Pavement Marking*

## PUBLIC CONNECTIONS

### Guidelines

- A pedestrian connection should be provided between all public spaces. This may be achieved through the use of sidewalks, trails, or connecting to the overall public sidewalk network.
- Connections shall be at least 6 feet in width.

## PEDESTRIAN CROSSINGS

### Guidelines

- Mid-block crossings for pedestrians should be indicated within the travel lanes by a change in material and/or texture, and not merely painted crosswalks. A change in elevation is appropriate as well.
- Signage for drivers should indicate a requirement to stop for all pedestrians attempting to cross.
- Consider innovative lighting for crosswalks including pedestrian area lighting unique to crosswalks and integrated street illuminations that visually mark the crosswalk.
- Flexible and reflective delimiters should be placed in the center of street pavement where it intersects with a pedestrian crossing.
- Enhanced design language should be embraced with crosswalks enhancing the public realm via distinct design, colors, materials, and designs appropriate to the various areas and art themes of the Downtown.

### CROSSWALK | MARKINGS



### CROSSWALK | ENHANCEMENTS

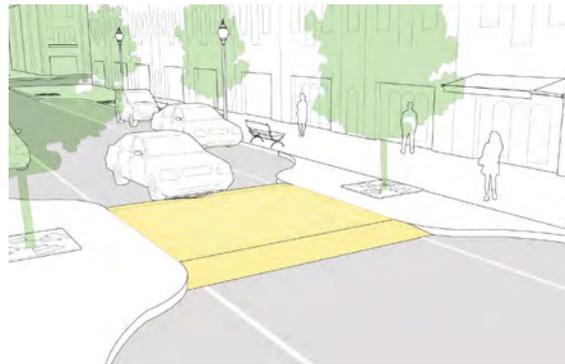


# CIRCULATION STANDARDS & GUIDELINES | ALL DEVELOPMENT

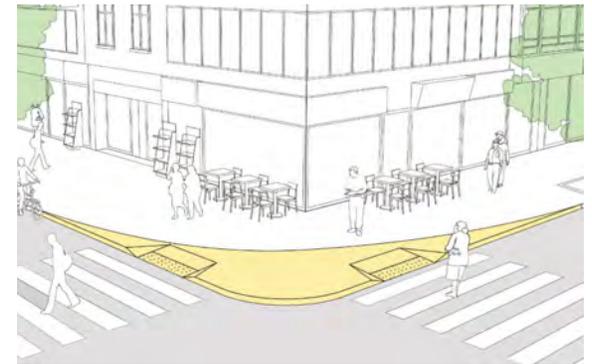
## TRAFFIC CALMING

Traffic calming measures should preemptively be taken to ensure streets are safe for pedestrians and remain accessible to vehicles. The following calming measures are recommended.

- Speed Tables
- Bumpouts and Curb Extensions
- Raised Crosswalks
- Speed Humps
- Narrow Travel Lanes
- Median Islands/Refuge Islands
- Textured Pavement
- Street Trees & Landscaping
- Bi-Directional Single Lanes
- Chicanes



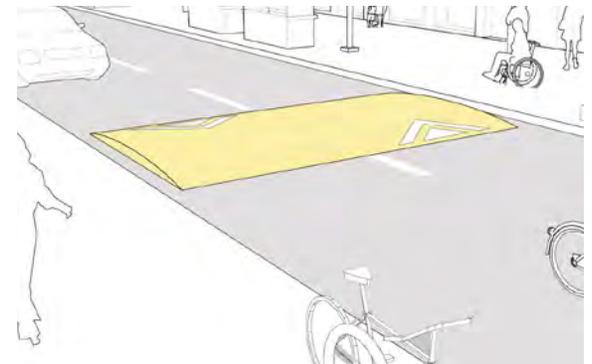
**Speed Table** - A large area of raised pavement requiring lower speeds to smoothly traverse.



**Bumpouts/Curb Extensions** - Narrows crossing distance for pedestrian and increased radius requires slower speed for turning vehicles.



**Raised Crosswalks**- Functioning similar to a speed table, this element improves pedestrian visibility.



**Speed Hump** - A raised area of pavement requiring lower speeds to traverse.



**Narrow Travel Lanes** - Reduced lane widths requires slower speeds to navigate.



**Median/Refuge Islands** - Narrows roadway and provides an area of protection for pedestrians crossing the street.



**Textured Pavement** - Signals drivers to reduce speed.



**Street Trees & Landscaping** - Visually narrows the perspective of street to encourage cautious driving.



**Bi-Directional Lanes** - A large area of raised pavement requiring lower speeds to traverse.



**Chicanes** - A serpentine path requiring reduced speed.



# SITE & LANDSCAPE STANDARDS | GUIDELINES **C**

---

# SITE & LANDSCAPE STANDARDS & GUIDELINES | ALL DEVELOPMENT

## GENERAL LIGHTING

### Guidelines

- Basis of Design
  - All pole lighting fixtures should be based on a lantern design.
  - Fixtures may lean towards traditional or contemporary design but must maintain a lantern basis.
- Lighting fixtures should generally minimize light trespass onto neighboring properties, reduce light pollution in the night sky, and mitigate glare.
- Light sources should be shielded from street view.
- Up-lighting should be minimized and generally reserved for tree canopies or key building features.
- Lighting should provide visual cues about important destinations or architectural features. These may include building entrances, pedestrian crossings, seating, play features, building recesses, sculpture/art, and roadway intersections.
- Efficiency & Maintenance
  - Reduce the number of lamp types and wattage to standard typology(s).
  - Luminaries should be easily accessible for maintenance.
  - Consider appropriate materials for site conditions.
  - Ensure any public light can be serviced by a bucket truck.
  - Ensure fixture components can be easily replaced/repared with tool-free lamp and ballast/driver access.

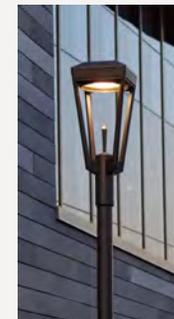
### Standards

- A. All lighting shall be warmer tones, a maximum of 4000 Kelvin (k).
  - i. Public spaces must be lit to a minimum of 2000k
  - ii. Public play areas may be lit up to a maximum of 5900k.
- B. All lighting shall have a minimum CRI of 80.
- C. All fixtures shall be configured to provide lighting from dusk to dawn.
- D. Plazas, seating areas, and landscaped drop zones shall be illuminated to support safe circulation.

- E. Pedestrian paths adjacent to buildings may utilize the building lighting and/or building mounted luminaries.
- F. House-side shields shall be used on streetlights adjacent to residential uses.



## LIGHTING | LANTERN DESIGN



## SEATING

### Guidelines

- A mix of seating options should be available, including the following:
  - Benches and chairs of various lengths, shapes, sizes, lounge angles, with and without backs and armrests, and with adjacent wheelchair and stroller space.
  - Low planters and seat walls with various seat heights made of brick, stone, pre-cast concrete, or stone veneer walls. Minimum seat depth of 18 inches.
  - Free-standing blocks/stones with smooth tops and eased edges that complement or match building materials.
  - Sculptural seating is encouraged, as it creates a visually stimulating element within space, and simultaneously offers a moment of informal play for children. Refer to the Westfield Art Master Plan for additional information and other playful sculptures.
  - Informal seating should be encouraged through the use of low walls, boulders, railings, and other opportunities for people to wait, rest, and relax.
  - Multi-functional furniture.
  - Kinetic seating, like swings or swivel chairs.
- Placement:
  - Site furnishings should be located in both areas of shade and full sun.

### Standards

- A. A minimum of 1 linear foot of seating shall be provided for every 40 square feet of public space.
- B. Seating specifically sized for children should be provided in any public space over 2,500 sf or contains a formal or informal play structure.
- C. 50% of loose furnishings should be lightweight so they can be easily moved by users to shift to their preferred seating location, or assemble into groups.



# SITE & LANDSCAPE STANDARDS & GUIDELINES | ALL DEVELOPMENT

## PUBLIC SPACE LANDSCAPE

### Standards

- A. A minimum of 30% of each public plaza or courtyard shall be shaded at the peak sun hour.
- B. A minimum of 30% of each public space shall be planted with landscape materials. Plants located within planters may count towards this requirement.

## LANDSCAPE MATERIAL

### Guidelines

- Groundcover – Should be planted densely to cover soil and mulch. Avoid planting directly over a tree's rootball.
- Use of suspended paving / soil cells is highly recommended for trees.
- Investigate opportunities for bioswales and internal stormwater storage that can provide for natural irrigation of trees.
- Soil should be high quality and nutrient rich to support plant life.
- Tree varieties - Species should be chosen based on their functional characteristics and intended use. Ideally, canopies should shade streets, trails, and other public spaces.
- Textures and color - A diverse range of leaf color, form, structure, and bark should be considered in all planting areas within the Downtown.
- Trees in parking lot islands should be large enough to adequately support the root zone.

### Standards

- A. Minimum Planting Soil Depths:
  - i. Turf: 6" - 8".
  - ii. Shrubs/Perennials: 24".
  - iii. Trees (in suspended paving soil cells): 36".
- B. Salt Tolerance – Areas bordering sidewalks, roadways, parking lots, or plazas that may receive winter de-icing salt and require plants with a higher salt tolerance.
- C. Clear Sightline – A clear sightline should be maintained at all times at street intersections. Landscape material must be trimmed clear between 3' and 6' in height.
- D. Tree Locations:
  - i. Street trees shall be located within the right-of-way at a spacing of 1 per twenty-five feet (25').
  - ii. Coordinate all underground utilities with tree locations and projected root zones.
- E. Tree Horticulture:
  - i. Trees should be permitted to grow to maturity for a full canopy.
  - ii. Topping of trees is not permitted – while this temporarily controls height and size of the tree, it damages the long-term health of the specimen.
  - iii. Trimming branches for truck and pedestrian clearance is appropriate as trees mature.
- F. Tree Species Characteristics Desired:
  - i. No trees that bear nuts, fruit, or seeds that will be a nuisance or potentially damage vehicles or harm pedestrians.
  - ii. No trees that are easily susceptible to wind or ice damage.
  - iii. Trees shall be tolerant to urban environment, poor soils, and exposure to salt.
  - iv. Native trees are preferred.
  - v. Do not use slow growing trees or trees that require significant and shallow root areas.
  - vi. Diversify species selection as much as possible to mitigate potential for future blight issues.
  - vii. Do not use invasive plants or plants with known critical disease or pest risks (e.g., Callery Pear or Ash).



## PROHIBITED PLANT SPECIES

### Trees

- American Elm - *Ulmus americana*
- Ash - *Fraxinus*
- Austrian Black Pine - *Pinus nigra*
- Autumn Olive - *Elaeagnus umbellate*
- Black Locust - *Robinia pseudoacacia*
- Boxelder - *Acer negundo*
- Callery Pear - *Pyrus calleryana*
- Cottonwood - *Populus deltoides*
- European White Birch - *Betula pendula*
- Lombardy Pear - *Populus nigra*
- Mimosa - *Albizia julibrissin*
- Mulberry - *Morus species*
- Norway Maple - *Acer platanoides*
- Princess Tree - *Paulownia tomentosa*
- Russian Olive - *Elaeagnus angustifolia*
- Siberian Elm - *Ulmus pumila*
- Silver Maple - *Acer saccharinum*
- Tree of Heaven - *Ailanthus altissima*
- White Poplar - *Populus alba*
- Willow - *Salix species*

### Shrubs

- Bicolor Lespedeza - *Lespedeza bicolor*
- Black Alder - *Alnus glutinosa*
- Buckthorn - *Rhamnus cathartica*
- Bush Honeysuckles - *Lonicera maackii, etc.*
- Privet - *Ligustrum*
- Multiflora Rose - *Rosa multiflora*
- Winged Burning Bush - *Euonymus alatus*

# SITE & LANDSCAPE STANDARDS & GUIDELINES | ALL DEVELOPMENT

## URBAN PLAY

### Guidelines

- Play opportunities should be considered in all public spaces, either formally or informally. Small playful sculptures provide informal opportunities for children in Downtown to explore their surroundings while parents may be shopping or dining nearby. Small informal opportunities also reduce the need for formalized playgrounds, which are not always conveniently located.
- Refer to the Westfield Art Master Plan for information on playful sculptures.
- Elements like urban swings are encouraged.

### Standards

- A. All formal play elements must be located on an appropriate surface per manufacturer's specifications. Loose fill surfacing such as loose mulches, pea gravel, sand, etc. shall not be used unless approved by the Planning Department.



## BICYCLE PARKING

### Guidelines

- Unique and sculptural racks are encouraged, provided they meet the standards below. Refer to the Westfield Arts Master Plan for information on sculptural bike racks.

### Standards

- A. Racks must be permanently mounted to the ground, building, or other permanent surface and provide at least two points to which a bicycle can be secured. Free standing racks are not permitted.
- B. Bicycle racks shall be provided at all buildings within 25' of the primary building entrance.
- C. All racks must accommodate at least two bicycles simultaneously.
  - i. A minimum of 8 exterior bicycle spaces (4 racks which can accommodate 2 bicycles each) are required per building.
  - ii. Independent racks must be separated by at least 28 inches.

### DESIGN | ACCEPTABLE



*Provides multiple lock points for bikes.*

### DESIGN | NOT ACCEPTABLE



*Does not meet standards.*

# SITE & LANDSCAPE STANDARDS & GUIDELINES | ALL DEVELOPMENT

## WAYFINDING

### Guidelines

- Placement - Wayfinding signage should be placed at decision points, intersections, and entry/exit points.
- Symbols, Typeface, and Colors - Should be standardized across the Downtown for consistent messaging and readability.
- Distance, approximate time of travel, and public services should be indicated to assist visitors with decision making. Example “Public Restrooms 2 Blocks Ahead.”
- Tactile maps of the Downtown are encouraged, and should be placed at regular intervals.

### Standards

- A. Accessibility - pictographs, icons, and text must have a high contrast with its background to promote visibility of information.
- B. Braille must be included on all pedestrian oriented signage that indicates permanent spaces or directions.



## PUBLIC ART

### Guidelines

- Public art should be in all Downtown Westfield’s public spaces.
- A public space is generally defined as an area that is open and accessible to members of the public. It can encompass various locations, including parks, streets, sidewalks, and plazas. Public spaces, which may be publicly owned, can also include privately owned areas that are accessible to the public, such as urban retail courtyards. The key characteristic of a public space is its accessibility and openness for the public to use and enjoy
  - The placement of public art is to be visible from adjacent public spaces to encourage people to travel between spaces on a journey of discovery.

### Standards

- A. Refer to the Westfield Arts Master Plan for detailed information and requirements.



## LIFESTYLE AMENITIES

### Standards

- A. Trash/Recycling shall be provided on the corner of every block.
- B. At least one trash/recycling receptacle shall be provided for every public space over 1,200 sf.
- C. Cigarette Disposal - should be provided in conjunction with trash and recycling receptacles.
- D. At least one pet station shall be provided in every public space over 1,500 square feet.
- E. At least one pet station shall be provided on every street frontage. One station may count for two separate streets if located on a corner.
- F. Pet stations must include a waste receptacle and a box of additional waste bags.



## PAVING MATERIALS

### Guidelines

- A change in ground plane materials should occur to indicate an entry point to buildings and outdoor public spaces.
- Textured materials or a pattern of pavers should be used to indicate activity areas - like seating and play elements - and to highlight circulation paths within public spaces.
- Colors should be used to accent special places, pathways, and zones within the public realm.
- Unique types of pavers or patterns may be used as larger ground plane art installations. See the Wesfield Arts Master Plan for additional guidelines and requirements.
- Use of permeable pavers is encouraged to reduce stormwater runoff.

### Standards

- A. Concrete must have a brushed or other textural finish. Polished concrete is not permitted for walking surfaces.





# PLAN AREAS

## STANDARDS | GUIDELINES

# D

---

## PLAN AREA STANDARDS & GUIDELINES | DOWNTOWN CORE

The Downtown core is characterized by a fusion of historic charm, modern appeal, and interconnected public spaces. Two to three story commercial buildings feature retail shops, offices, and small restaurants that complement adjacent residences. New housing includes 3-5 story mixed-use buildings with ground floor retail and apartments above. Shared parking garages support the needs of residents and visitors alike. Plazas and other open spaces provide park-like settings, creating a dynamic balance between nature and Downtown. These elements form a cohesive and lively Downtown area where commercial vitality, residential comfort, architectural beauty, and natural tranquility coexist.

### ARCHITECTURE

#### Standards

- A. Minimum Lot Size - 4,800 sf.
- B. Front Setbacks:
  - i. SR 32 E North side:
    - a. 0' for building adjoining existing structures.
    - b. 10' Minimum - 15' Maximum.
  - ii. SR 32 E South Side:
    - a. 10' Minimum - 15' maximum.
- C. Materials - An accent material/color may be used for no more than 20% of a facade.
- D. Minimum Height - 2 stories.
- E. Maximum Height - 6 stories.
- F. Penn Street Height:
  - i. All buildings along Penn Street between Union Street and Westfield Boulevard are limited to 3 stories for the first 35' in depth from the ROW.
- G. Residential Density - 15-50 units per acre.
- H. Materials:
  - i. Glazing:
    - i. 60% min. required on the ground floor for commercial spaces fronting on public streets or plazas.
    - ii. 3-5% of total glazing area on ground floor

#### DOWNTOWN CORE AREA



may be covered by permanent and/or temporary signage.

I. Shelter from Elements:

- i. Awnings or other shade/protective structures are required for all buildings with frontages along SR 32. This covering is required for at least 40% of the building frontage along the ground floor.
- ii. Features must have a minimum depth of 5' to satisfy this requirement. Second floor balconies that project more than 5' and are no more than 14' above grade may be counted towards this requirement.

## CIRCULATION

### Guidelines

- Pedestrian protection from the street is highly encouraged through the following methods:
  - Decorative rails - These rails are intended to discourage pedestrians from crossing SR 32 E at locations other than dedicated crossing. These may also informally provide a space to sit or lean. Openings in rail should be no larger than 4". These are not intended to stop a vehicle.
  - Planters.
  - Bollards.



## PLAN AREA STANDARDS & GUIDELINES | MAIN STREET

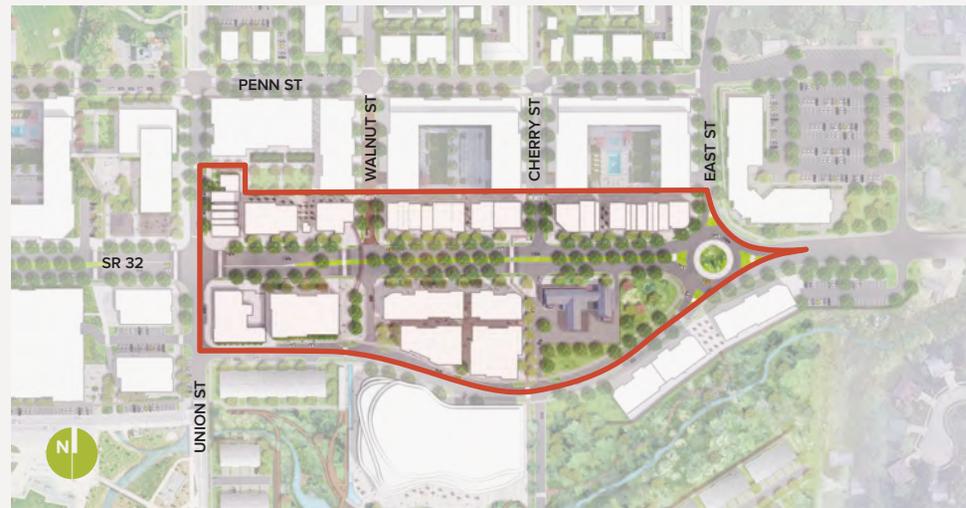
Re-establishing the Downtown main street creates a vibrant core that blends tradition with modern needs. Two to three-story buildings frame the street, evoking historic character while providing new opportunity for commercial growth. Public spaces are thoughtfully interspersed, offering plazas and gathering spots that invite community interaction and connectivity. Ground floor restaurants and retail spaces, activate the streetscape while upper levels provide flexible office space. Together, these elements restore main street's role as the civic and cultural heart of Downtown life.

### ARCHITECTURE

#### Standards

- A. Minimum Lot Size - 4,800 sf.
- B. Setbacks:
  - i. SR 32 E North side:
    - i. 0' for building adjoining existing structures.
    - ii. 10' Minimum - 15' Maximum.
  - ii. SR 32 E South Side:
    - i. 10' Minimum - 15' maximum.
- C. Materials:
  - i. Brick - Brick, stone, or equivalent material, as determined by the Community Development Director is required on 100% of the facade facing SR 32.
  - ii. An accent material/color may be used for no more than 20% of a facade (e.g. metal, terracotta, other decorative accent material).
- D. Minimum Height - 2 stories.
- E. Maximum Height - 3 stories.
- F. Residential Density - 15-50 units per acre.
- G. Glazing:
  - i. 60% min. required on the ground floor for commercial spaces.
  - ii. 3-5% of total glazing area on ground floor may be covered by permanent and/or temporary

#### MAIN STREET AREA



signage.

H. Shelter from Elements:

- i. Awnings or other shade/protective structures are required for all buildings with frontages along SR 32 E. This covering is required for at least 55% of the building frontage along the ground floor.
- ii. Features must have a minimum depth of 5' to satisfy this requirement. Second floor balconies that project more than 5' and are no more than 14' above grade may be counted towards this requirement.



## CIRCULATION

### Guidelines

- A. Pedestrian protection from the street is highly encouraged through the following methods:
- i. Decorative rails - These rails are intended to discourage pedestrians from crossing SR 32 E at locations other than dedicated crossing. These may also informally provide a space to sit or lean. Openings in rail should be no larger than 4". These are not intended to stop a vehicle.
  - ii. Planters.
  - iii. Bollards.



## PLAN AREA STANDARDS & GUIDELINES | MAIN STREET

Building facades within the Main Street Plan Area should be of a design and character that contributes to the feeling of a historic Midwest main street. The buildings should include historic character and feature, where possible and practical drawing influence from the history of Westfield. Facade materials shall be of enduring, timeless materials like brick, stone, glass, and metal. Detailing should emphasize the base-middle-top style of traditional design.

### MAIN STREET PLAN AREA ARCHITECTURE | BUILDING DESIGN ELEMENTS



#### **Building Cornice**

Decorative top of building, practical use to keep water away from the face of the building and transition from the materials used on the finish wall to the roof.

#### **Windows — Punched Openings**

Fabric buildings (defined in the caption below) typically have a simple wall plane with punched openings and double hung windows.

#### **Storefront Cornice with Signage**

The storefront cornice divides the retail portion of the building and the residential or office upper floors while also providing a place for store signage. If using an awning, attach it below the storefront cornice so signage remains visible. Also if using an awning, specify a deep one, minimum 8".

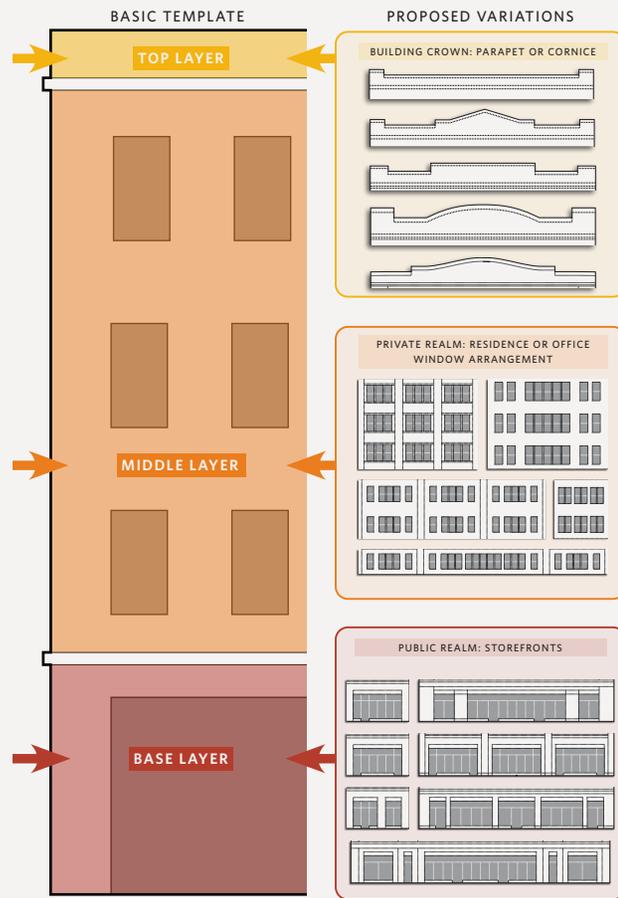
#### **Shop Display Windows**

Display windows come in all configurations but will typically have a low panel for protection from foot traffic, a large display area, and transom windows above.

#### **Outdoor Seating**

Storefronts scaled to people attract people. For restaurants, this presents the opportunity for outdoor dining and seating areas.

# MAIN STREET PLAN AREA ARCHITECTURE | BUILDING DESIGN ELEMENTS



## PLAN AREA STANDARDS & GUIDELINES | LEGACY AND HISTORIC CORE

This area is intended to retain much of the historical character of Downtown Westfield as it contains several contributing buildings of the Historic District. If feasible, other historical buildings within Downtown may be relocated to this area. Homes and buildings should retain the historical character of the time that they were built. A public plaza with historical displays should be developed within this area.

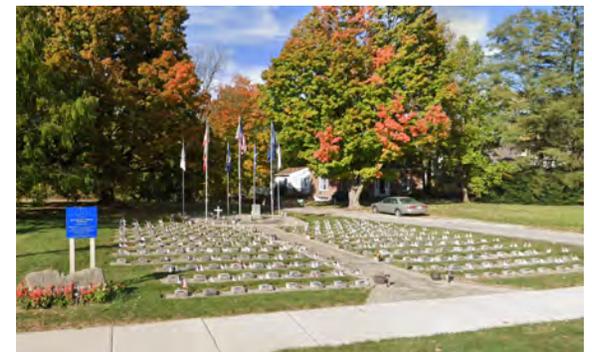
### ARCHITECTURE

#### Standards

- A. Building height to be a maximum of 3 stories.
- B. Minimum Lot Size: 5,000 sf.
- C. Minimum Lot Width: 50'.
- D. Residential Density – 4 to 15 units per acre.
- E. Setback – New buildings in this area should be setback within 5' of the average setback of the two adjacent buildings on either side of the new development.
- F. Residential garages shall be setback at least 10' behind the front facade of the primary entry.
- G. Building form and material shall generally be consistent with the existing form and materials of buildings listed as Contributing within the Westfield Historic District, as determined by the Community Development Director.

#### LEGACY AREA





## PLAN AREA STANDARDS & GUIDELINES | RESIDENTIAL AREA - INFILL

Residential infill areas are envisioned to provide a mix of medium-density residential types—accessory dwelling units, duplexes, triplexes, townhomes, and multi-family small—creating a neighborhood with variety and character. These new homes support a balanced density and provide a Downtown with a crucial population to support local businesses. Interconnected open spaces and pedestrian pathways connect these areas with Asa Bales Park and the rest of Downtown, integrating recreation and nature into daily life.

### ARCHITECTURE

#### Standards

- A. Residential Density – 8 to 25 units per acre.
- B. Maximum Height – 3 stories.
  - A. 4 stories permitted for buildings along the north side of Penn Street.
- C. Lot Size:
  - i. Minimum Area – 1,500 sf.
  - ii. Minimum Width – 25’.
- D. Setbacks:
  - i. Front – 5’ minimum.
  - ii. Side – 5’ minimum.
  - iii. Rear – 5’ minimum.
- E. Appropriate building typologies:
  - i. Single-family detached.
  - ii. ADU.
  - iii. Townhome.
  - iv. Duplex/Triplex.
  - v. Multi-Family Small

### RESIDENTIAL - INFILL AREA





## PLAN AREA STANDARDS & GUIDELINES | RESIDENTIAL AREA - CREEKSIDE

The Grassy Branch corridor is envisioned as a vibrant part of Downtown that blends ecology, housing, and recreation. New single-family and multifamily housing along the creekway provides a unique living opportunity in the heart of Downtown. Riparian enhancements restore native plantings, while trails, overlooks, and gathering spaces activate the creek's edge, encouraging recreation and community life. Together, these improvements transform the creek into a central asset that defines Downtown's future.

### ARCHITECTURE

#### Guidelines

- Development along the creekway should include creative interaction with the natural area. This can include activated retail tenant spaces that overlook the creekway, and other interesting pathways or connections to the boardwalks.
- Parking strategies should be innovative and hide vehicles from the creekway users.

#### Standards

- A. Residential Density – 8 to 25 units per acre.
- B. Minimum Height – 2 stories
- C. Maximum Height – 5 stories.
- D. Lot Size:
  - i. Minimum Area – 1,500 sf.
  - ii. Minimum Width – 25'.
- E. Setbacks:
  - i. Front – 5' minimum.
  - ii. Side – 5' minimum.
  - iii. Rear – 5' minimum.

### RESIDENTIAL - CREEKSIDE AREA





# PLAN AREA STANDARDS & GUIDELINES | GRAND JUNCTION DISTRICT

The Grand Junction Plan Area is defined by its connection to Grand Junction Plaza and its vibrant restaurant row along Park Street. Existing single-family homes and businesses are interspersed through the area. Additional medium to high density infill housing is anticipated for this area, as are additional 1 to 2 story commercial spaces. Together with Grand Junction Plaza, this forms a charming portion of Downtown life where modern amenities meets hometown charm.

## ARCHITECTURE

### Standards

- A. Minimum Lot Size: 7,800 sf.
- B. Building Height:
  - i. Jersey Street Frontage - 4 story maximum.
  - ii. Park Street Frontage - 3 story maximum.



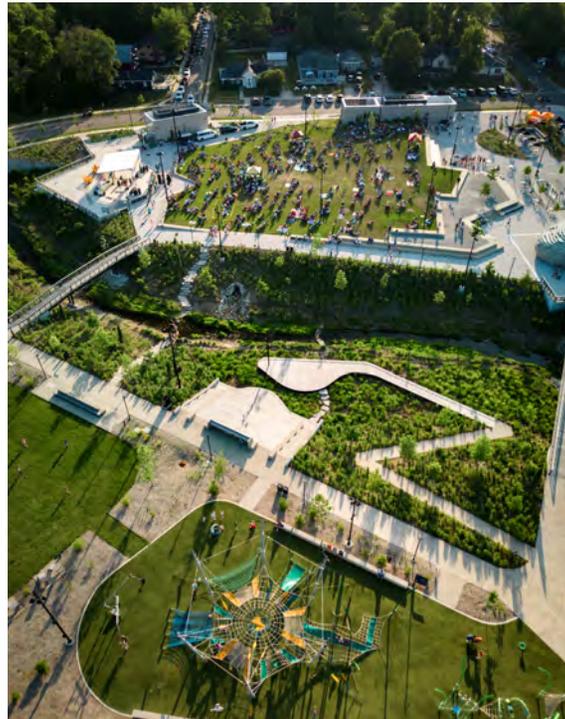
## GRAND JUNCTION AREA



## CIRCULATION

### Guidelines

Walking trails/paths and other connectivity elements are highly recommended between developments.



## PLAN AREA STANDARDS & GUIDELINES | DOWNTOWN ADJACENT

Over the coming decades - and as the community continues grow - areas immediately adjacent to the Downtown present natural opportunities for continued redevelopment. This redevelopment plan does not have a specific vision for these transitional areas, but it is important to note that these are areas where growth is likely to occur. Redevelopment in these areas would likely include additional infill and medium-density housing, or supporting commercial uses to the Downtown. By thoughtfully planning future development - like maintaining walkable connections - these areas can extend the vibrancy of Downtown, and ensure growth occurs in a cohesive manner that strengthens the overall community.

### ARCHITECTURE

#### Guidelines

Development is encouraged to use the guidelines and standards of the Residential Infill Plan Areas of Downtown and apply them to these areas. Infill housing, mixed density, high quality materials, and appropriate scale should all be considered.

Appropriate building typologies:

- i. Single-family detached.
- ii. ADU.
- iii. Townhome.
- iv. Duplex/Triplex. (Except fronting SR 32)
- v. Multi-Family Small.

#### Standards

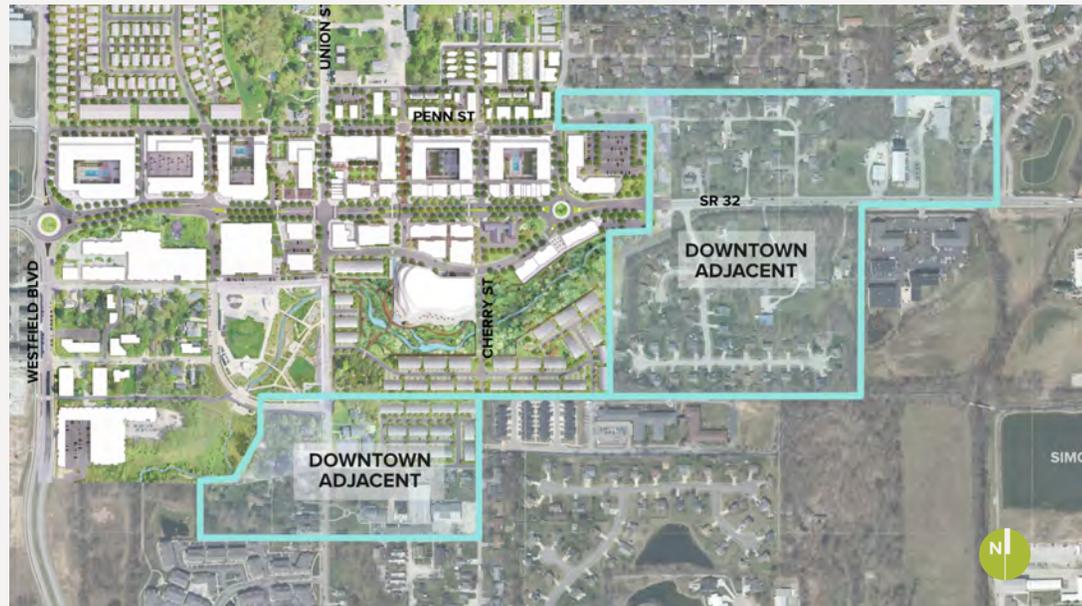
- A. Residential Density – 4 to 25 units per acre.

### CIRCULATION

#### Guidelines

Walking trails and other connectivity elements is highly encouraged between developments.

### DOWNTOWN ADJACENT AREA





## PLAN AREA STANDARDS & GUIDELINES | WESTFIELD BOULEVARD

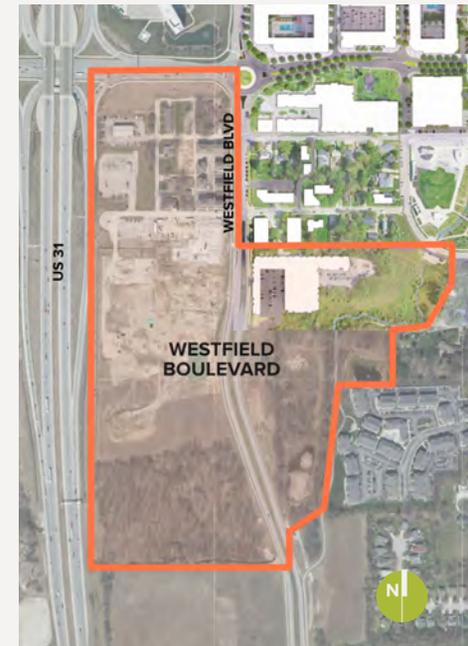
Located along US-31 and at the intersection of Westfield Boulevard and Park Street, the Westfield Boulevard area serves as a mixed-use hub, combining civic, commercial, medical, and residential use near the heart of Westfield. Anchored by the Public Library, and Township Trustee offices, the Area will host a medical office building and ambulatory surgery center - enhancing local healthcare access - and high-quality housing options— townhomes, and apartments—providing an additional residential presence. All of these uses are supported by structured parking. The Area is linked to Grand Junction Plaza and the rest of Downtown by the Midland Trace Trail.

### CIRCULATION

#### Guidelines

Walking trails and other connectivity elements highly recommended between developments.

#### WESTFIELD BLVD. AREA





## NOTABLE AREA STANDARDS & GUIDELINES | GRASSY BRANCH CREEKWAY

As a crucial part of the overall Downtown redevelopment, it is important to lay out general guidelines and standards for how the creekway area should be enhanced. The purpose of these guidelines and standards is to lead creekway revitalization efforts in a manner that balances ecological restoration and community recreation.

### CIRCULATION

#### Guidelines

- Raised boardwalks should be the primary built trail through the creekway area.
- Naturalistic trail options of mulch, mowed grass, soil, or fine gravel are encouraged, though may require regular maintenance.

#### Standards

- A. All creekway paths must be a minimum 6' wide.
- B. All boardwalks and other raised circulation routes shall have a 2" high raised edge for the purpose of preventing roll-of stroller or wheelchair wheels, and to provide a edge indicator for the visually-impaired.



### GRASSY BRANCH CREEKWAY



#### Notes

- Any work or changes to the creekway area will require IDNR collaboration and permits.
- Remapping of floodway may be required.
- Buildings shown are conceptual, and will require proper permitting.

## ECOLOGY

### Guidelines

- Remove invasive species.
- Use plant species that will stabilize stream banks to mitigate erosion over time. Erosion control mats, fabric, or vegetative rolls may also be considered.
- Select plant material that can withstand strong current flows. Root material should be fibrous and substantial but lack a tap root.
- Tree planting:
  - Provide a variety of native species.
  - Plan for seasonal variety with a spectrum of spring blooms and fall colors.
- Shrubs should be interspersed between planted trees, and be of mixed species.

### Standards

- A. Size at Planting - minimum size at planting, as defined by the Westfield UDO shall apply.
- B. Saplings should be planted between September 15 and December 15, or until the ground has frozen; or between the last spring frost and June 1.
- iii. Plant Selection - A single species should not make up more than 20% of the total planted.

## AMENITIES

### Guidelines

- Signage:
  - Interpretive signage regarding creek ecology or plant/wildlife education is encouraged.
  - Wayfinding signs should be provided at trail intersections to provide general direction and path to exit the creekway.
- Seating:
  - Provide a variety of seating options. This may include natural elements such as boulders and logs. Both formal seating attachments or natural condition of log/boulder is acceptable.
  - Seating can be playful and sculptural in design, evoking aspects of the creek itself.
- Art - interactive sculptures are encouraged and should be interspersed throughout the area. Reference the Westfield Arts Master Plan for further detail.
- Pedestrian bridge:
  - Inspired by arched stone bridges
  - Spans approximately 150' over Grassy Branch from the northern overlook to Old Friends Cemetery Park.

### Standards

- A. Seating - shall be provided every 120' along all trails through the creekway area.







**ACKNOWLEDGMENTS**

APPENDIX

## TO THE CITY OF WESTFIELD,

Browning Day is exceptionally grateful for the opportunity to work with you on this new and bold vision for the next phase of Downtown. We embarked on this journey optimistic and enthusiastic about the future of the City of Westfield and that light has not diminished in the slightest as we've come to know the great leaders, invested citizens, and good stewards of the City and its rich history. From the Historical Society's deep knowledge of the stories of the community to Mayor Willis' team leading the charge to think boldly about what this City is and what it can become, the people of Westfield are all-in on their home.

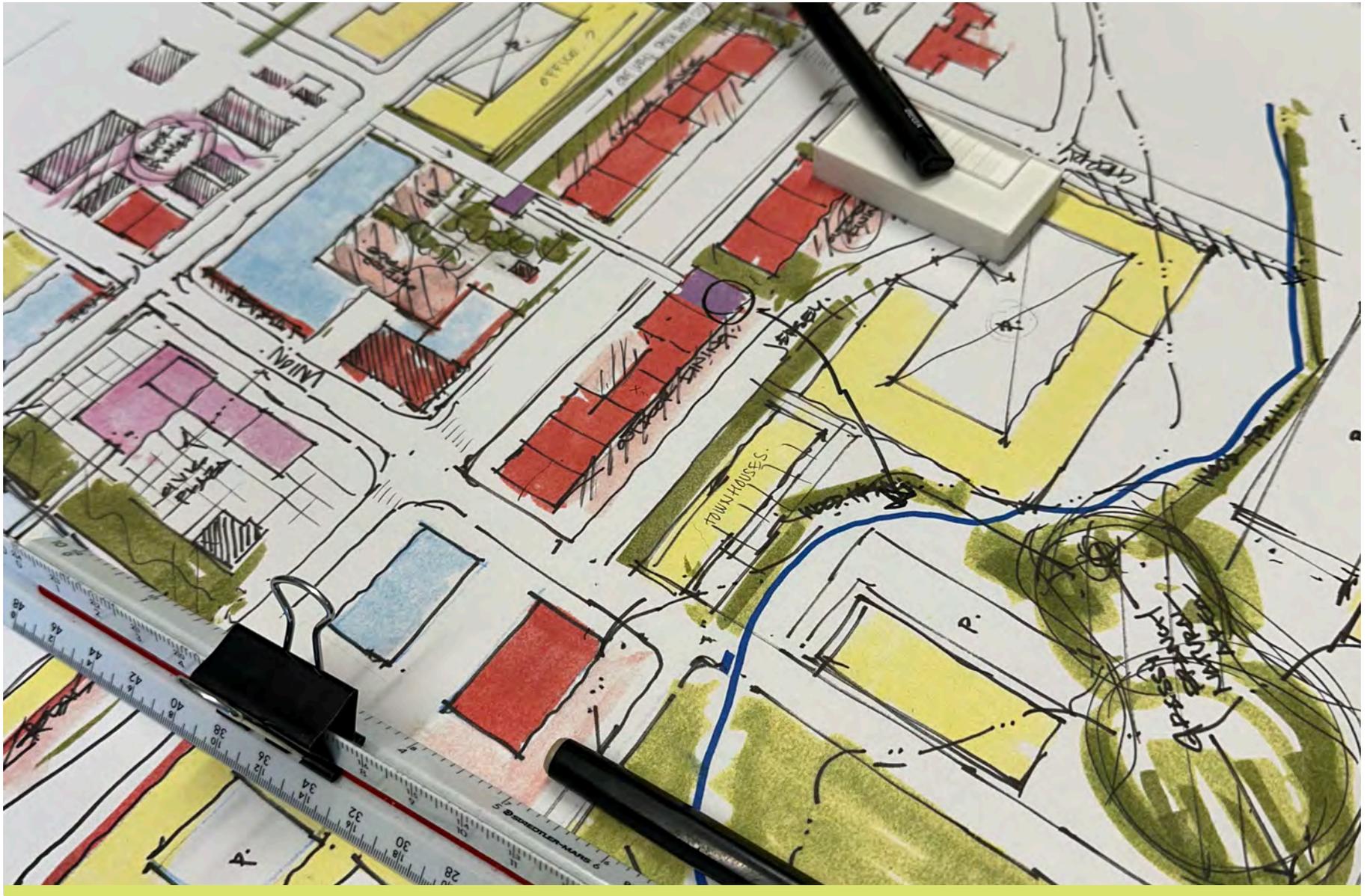
Browning Day is proud to stand with the City and its people at the start of the next chapter of their story. This Vision for Downtown is bold, considered, and most importantly, achievable. The plan is a transformation to the downtown of the city that combines traditional charm, the history of place, and unique spaces to live and belong.

We're tremendously proud of the work we've achieved together, and we look forward to walking with you to support your vision as the plan moves from Idea to Reality.

With Care,

A handwritten signature in black ink that reads "Greg Jacoby". The signature is stylized with a long horizontal line extending to the left of the name.

Greg Jacoby, AIA  
Executive Principal, Chief Executive Officer



## ACKNOWLEDGMENTS

First and foremost, acknowledgments are due to Mayor Scott Willis for his vision for the City of Westfield and commitment to bringing that vision to life. He constantly challenged the design team and city project team to think bigger with their concepts; to see that Westfield can and will be a leading American City in the next 20 years. His leadership is the major force behind this Vision for Downtown.

Backing up Mayor Willis are Deputy Mayor Danielle Carey-Tolan and Director of Economic Development Jenell Fairman who are equally responsible for ensuring that the Mayor's bold vision for the future is realistic and achievable. They provided invaluable support and feedback to the project team at critical moments.

At the City of Westfield, the seamless team leadership and coordination of Caleb Ernest allowed the project team to focus on the critical research, ideation, and visioning processes required to bring this plan to life.

The value of every team member at the City of Westfield has been critical to this process. Often key ideas and concepts would come from outside a particular team member's area of expertise and that confidence and freedom to share ideas brought about a much more considered and relevant downtown plan. In particular, thanks to Kevin Todd, Johnathon Nail, Weston Rogers, Christopher McConnell, and Lauren Wahl who all contributed greatly to the success of this plan.

To all our community partners but in particular, Diana Peyton and the Westfield Historical Society for their knowledge and enthusiasm for the stories of the City and for their recommendation of "*Our Westfield*."

To the Notre Dame Housing and Community Regeneration Initiative, Marianne Cusato, and Mayor James Brainard for their review and feedback of the Redevelopment Plan and their many, many sources decrying pedestrian bridges. Also to Rundell Ernstberger Associates, Cynthia Bowen, and Carl Kincaid for their coordination on the Comprehensive Plan and the Park Street design.

And finally at Browning Day, special recognition to some our team members. First and foremost, our Director of Business Development Michelle Freeman, who often goes unsung in our praises but who puts us all in a position to succeed. To our Landscape Architects Tom Green and Mia Dalton for their creativity and craft. To Drew Braley for his eagle eyes. To Charles Russell for leading the planning charge throughout the night. To Paul Wolfe for his vision for this plan and leadership in innovative technologies. And finally, to Christine Pearson for guiding the project to success, the team owes ultimate success to her hard work and dedication.

## ACKNOWLEDGMENTS

### **CITY OF WESTFIELD**

<b>Scott Willis</b>	Mayor
<b>Danielle Carey-Tolan</b>	Deputy Mayor
<b>Jenell Fairman</b>	Director of Economic Development
<b>Caleb Ernest</b>	Long-Range Planning Manager, Project Manager
<b>Kevin Todd</b>	Director of Community Development
<b>Johnathon Nail</b>	Director of Public Works
<b>Christopher McConnell</b>	Director of Parks and Recreation
<b>Weston Rogers</b>	Senior Planner
<b>Lauren Wahl</b>	Redevelopment Strategist

### **BROWNING DAY**

<b>Greg Jacoby, AIA</b>	Principal In Charge
<b>Christine Pearson, PLA</b>	Senior Project Manager
<b>Charles Russell</b>	Urban Designer / Master Planner
<b>Paul D. Wolfe, AIA, RID</b>	Senior Design Architect
<b>Drew Braley, PLA, ASLA</b>	Director of Landscape Architecture
<b>Tom Green, PLA</b>	Senior Project Manager

### **HWC ENGINEERING**

<b>Scott Wilkinson</b>	Director of Transportation
<b>Kevin Miller</b>	Traffic Engineering Manager

### **SKENDER**

<b>Brian Simons</b>	Senior Vice President
---------------------	-----------------------

## **PROJECT TEAM**

---



# B

**DEVELOPMENT DATA**  
APPENDIX

Area	Key	Importance	Building Type	Building Location	Parcel Ownership	Stories	Total Bldg SF (est.)	Units	Site Area SF	Parking Spaces	Street Parking	Duration			Notes
												Development	Design   Approval	Construction	
<b>W. Penn St. Block</b>															
	01		Mixed-Use Residential	SR 32 E and Westfield Blvd. Roundabout, NE Corner		4	232,759	194	127,400	--	26				3 Stories along Penn Street
	01-R		Mixed-Use Retail (Penn Extension facing)	SR 32 E and Westfield Blvd. Roundabout, NE Corner		1	22,450	--	--	--	--				
	01-G		Mixed-Use Garage	SR 32 E and Westfield Blvd. Roundabout, NE Corner		3	166,500	--	--	349	--				
	02		Office (and Garage)	SR 32 E and Westlea St. NW Corner		4	185,912	--	80,414	350	31				3 Stories along Penn Street, Garage Area Not Included in Building Total
<b>Carnegie Plaza Block</b>															
	03	Catalyst	Mixed-Use Residential	SR 32 E and Westlea St. NE Corner		5	145,718	121	82,140	--	25				
	03-R	Catalyst	Mixed-Use Retail	SR 32 E and Westlea St. NE Corner		1	23,023	--	--	--	--				
	03-G	Catalyst	Garage	SR 32 E and Westlea St. NE Corner		4	104,367	--	--	363	--				
	04	Catalyst	Public-Private Office Building	SR 32 E and Union Street. NW Corner		5	75,000	--	82,650	--	14				
	04-P	Key	Public Plaza	Mid-Block, SR 32 E to W Penn St. Extension					46,165						
<b>Bank Block</b>															
	05		Existing, Bank Building Reuse	SR 32 E and Union St. NE Corner		2	7,870	--	11,785	--	4				Estimated building SF
	06		Office-S	SR 32 E East of Union; Mid-Block to Walnut St.		3	27,570	--	18,640	--	--				
	06-R		Office-S, Retail Tenant	SR 32 E East of Union; Mid-Block to Walnut St.		1	13,785	--	--	--	--				
	07	Catalyst	Public Plaza	Center of Block					22,645						
	08		Office-NW	E Penn St. and Union St. SE Corner		3	54,222	--	29,360	--	12				
	08-R		Office-NW, Retail Tenant	E Penn St. and Union St. SE Corner		1	15,300	--	--	--	--				
	09		Office-NE	E Penn St. and Walnut St. SW Corner		3	29,532	--	16,830	--	9				
	09-R		Office-NE, Retail Tenant	E Penn St. and Walnut St. SW Corner		1	14,766	--	--	--	--				
<b>Main St. North Block</b>															
	10		Retail Main Street 1	SR 32 E and Walnut St. NW Corner		2-3	25,797	--	11,400	--	--				
	11		Retail Main Street 2	SR 32 E and Cherry St. NE Corner		2	13,456	--	7,400	--	--				
	12		Retail Main Street 3	SR 32 E and Cherry St. NW Corner		2	12,426	--	7,100	--	--				
	13		Retail Main Street 4	SR 32 E and East St. Roundabout, NW Corner		2	21,572	--	11,730	--	--				
	14		Mixed-Use Multifamily	E Penn St. and Cherry St. SE Corner		4	96,900	81	65,435	--	17				3 Stories along Penn Street
	14-R		Mixed-Use Retail	E Penn St. and Cherry St. SE Corner, Cherry/Walnut Facing		1	15,600	--	--	--	--				
	14-G		Garage 1	E Penn St. and Cherry St. SE Corner		5	124,875	--	--	428	--				
	15	Catalyst	Mixed-Use Multifamily	E Penn St. and Cherry St. SW Corner		4	96,900	81	65,845	--	14				3 Stories along Penn Street
	15-R	Catalyst	Mixed-Use Retail	E Penn St. and Cherry St. SW Corner, Cherry/East Facing		1	15,600	--	--	--	--				
	15-G	Catalyst	Garage 2	E Penn St. and Cherry St. SW Corner		5	124,875	--	--	428	--				
	16	Catalyst	[Planned] Ambrose on Main	SR 32 E and East St. Roundabout, NE Corner		4	119,500	87	119,710	184	--				
<b>Main St. South</b>															
	17		Retail Main Street 1	SR 32 E and Walnut St. SW Corner		2	26,802	--	21,600	--	6				
	18		Retail Main Street 2	SR 32 E and Walnut St. SE Corner		2-3	21,690	--	14,950	--	--				
	19		Retail Main Street 3	SR 32 E and Cherry St. SW Corner		2	13,338	--	8,700	--	2				
	20		Jersey Street Retail 4	Jersey St. and Walnut St. NE Corner		2	24,988	--	13,565	--	8				
	21		Jersey Street Retail 5	Jersey St. and Cherry St. NW Corner		2-3	24,540	--	13,750	--	9				
	22		[Existing] Retail	SR 32 E and Union St. SE Corner		1	9,507	--	--	--	8				Estimated building SF, included telecomm
	23		[Existing] Field Brewing	SR 32 E and Cherry, SE Corner		1	10,458	--	--	--	14				Estimated building SF
<b>Creekside North</b>															
	24		Mixed-Use Multifamily	Jersey St. and Cherry St. SW Corner		5	163,014	136	99,350	--	12				
	24-R		Mixed-Use Retail	Jersey St. and Cherry St. SW Corner		1	15,900	--	--	--	--				
	24-G		Mixed-Use Garage 1	Jersey St. and Cherry St. SW Corner		4	159,500	--	--	404	--				
	25		High-End Townhouse Podium	Union St and Jersey St. SE Corner		3	42,000	19	21,700	20	--				Shared Subgrade Parking Assumed
	26	Key	Trail and riparian enhancements	Creekside, multiple routes					134,285	--	--				
	27	Key	Promenade and pedestrian bridge	North/south connector from Midland Trace Trail to Jersey Street					--	--	--				
<b>Union Square Block</b>															
	28		[Under construction] Union Square			6	273,350	196	91000	303	39				
	29		[Under construction] Sun King Union Square			2	14,458	--	20,908	12	22				
	30		[Planned] Union Square Office			3	31,200	--	10,000	TBD	TBD				
<b>Jersey 32 Block</b>															
	31		Jersey 32 Development			5	113000	216	163,693	530	TBD				
<b>Creekside South</b>															
	32		Residential	Union St. Eastside		3	Varies	11	43,500	--	TBD				Estimated Unit Count
	33		Residential	Midland Trace Trail, Northside		3	Varies	27	91,000	--	TBD				Estimated Unit Count

<b>Cherry St. Neighborhood</b>												
--		Mixed Residential Infill	Cherry Street, Mid Block South, East Side	2 or 3	--	25	252,700		TBD			Estimated Unit Count
<b>Westlea Neighborhood</b>												
34		Mixed Residential Infill - Parkside/ Penn Townhouses	West Side of Asa Bales Park, Northside of Penn St. Extension	3	--	36	120,300		TBD			Estimated Unit Count
35		BTR/BTO Single Family Detached	West Side of Westlea St., North of Penn St. Extension	3	--	112	767,000		TBD			Estimated Unit Count
<b>Legacy Neighborhood</b>												
--		Mixed Residential Infill - Various					--					
<b>East St. Neighborhood</b>												
--		Mixed Residential Infill - Various					--					
<b>Grand Junction Park</b>												
--		[Planned] Grand Junction Park Phase 2					--					
<b>Park Block</b>												
36		[Planned] Park and Poplar		1 to 5	TBD	325 max	462,100	90+	TBD			
--		[Existing / Planned] Restaurant Row					--					
<b>Roadway</b>												
--		[Planned] Jersey St					--					
--		[Under Construction] SR 32 E					--					
--		[Planned] Park Street Improvements					--					
<b>TOTALS</b>				-		<b>All Downtown Development</b>						
							Office	2,932,215	1,667	3,461	272	
							Retail	297,236	1,437	Garage	3,155	
								362,426	121			

**NOTE: ALL DATA SHOWN IS ESTIMATED BASED ON THE REDEVELOPMENT PLAN AND AVAILABLE PUBLIC AND FURNISHED REPORTS, RECORDS, AND PLANS. SUBJECT TO CHANGE AND ERROR BASED ON EXISTING CONDITIONS, CHANGES IN PLAN, AND OTHER FACTORS OUTSIDE THE BOUNDS OF THIS DOCUMENT.**





**INVENTORY AND ANALYSIS**  
APPENDIX

# WESTFIELD DOWNTOWN REDEVELOPMENT PLAN INVENTORY & ANALYSIS



January 29, 2025

**CONFIDENTIAL DRAFT**  
NOT FOR PUBLICATION  
PLAN IN PROCESS

CONTENTS SUBJECT TO CHANGE AS PLAN PROGRESSES THRU PROCESS

<b>CONTENTS  </b>	<b>INTRODUCTION</b>			
	WHAT WHY HOW		4	
	LOCATION & REGIONAL CONTEXT		5	
<i>FUTURE PAGES</i>	<b>PART I VISION - INTEGRATED VISION MAP</b>			
	VISION PLAN			
	BIRDSEYE			
	CHARACTER VIGNETTES			
	PLANNING AREAS			
	PLANNING AREA DETAILS			
	CONNECTIVITY FRAMEWORK			
<i>FUTURE PAGES</i>	<b>PART II HORIZON - GOALS OBJECTIVES ACTIONS</b>			
	IMPLEMENTATION/PHASING			
	CATALYST PROJECT(S)			
	KEY PROJECTS			
	NEAR HORIZON - WITHIN 5 YEARS			
	HORIZON - 5 TO 10 YEARS			
	VISION GUIDELINES			
	PLAN MAINTENANCE			
	<b>PART III ORIGIN - INVENTORY &amp; ANALYSIS</b>			
	DOWNTOWN DEFINED		6	
	EXISTING LAND USES		7	
	EXISTING ZONING		8	
	HISTORIC DISTRICT & ASSETS		9	
	CONNECTIVITY		10	
	STREET & INTERSECTION PERFORMANCE		11	
	NATURAL FEATURES		12	
	FLOODPLAIN		13	
	ENVIRONMENTAL FEATURES		14	
	CORRIDOR IMPACTS		15	
	DOWNTOWN ACTIVITY		17	
	SCALE STUDIES SUMMARY		18	
	SCALE STUDIES		19	
	GAP ANALYSIS SUMMARY		22	
	GAP ANALYSIS A-D		23	
	<b>PART IV - APPENDIX</b>			
	SURFACE & STORM WATER FEATURES REPORT			



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | TABLE OF CONTENTS**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 3

## INTRODUCTION

### WHAT

This Redevelopment Plan is a vision for the growth of downtown Westfield over the course of the next two decades. This strategic document is created with input from community members, local businesses, government officials, and other stakeholders resulting in a shared vision for the future of downtown. The Redevelopment Plan includes a comprehensive, long-term blueprint that outlines the goals, strategies, and actions to execute this vision.

### WHY

A vision plan is a roadmap for the future, guiding development and growth in a manner consistent with the needs, wants, and values of the community. It ensures that growth and change are planned, cohesive, and aligned with the values of the people who live and work there. By having a clear vision, cities can make wise decisions about investments, attract funding, and guide development in a direction that benefits the community as a whole.

### HOW

Engaging the community—residents, business owners, and other stakeholders—to gather input on what they want for downtown. Based on the input from the community and the analysis of existing conditions, planners set specific measurable goals that align with the desires of the community and the broader vision for the future. The final plan is adopted and guides development and policy-making.

### NOW WHAT?

To make this Vision Plan a reality, two critical steps need to be taken: 1) working with developers, businesses, and community groups to bring the vision to life, and 2) monitor progress and adjust the plan accordingly based on results.

*NOTE: This plan is a guide and subject to change.*

### WHO

#### CITY OF WESTFIELD

Mayor Scott Willis  
Westfield City Council  
Jenell Fairman  
Kevin Todd  
Caleb Ernest  
Weston Rodgers

#### BROWNING DAY

Christine Pearson  
Greg Jacoby  
Paul Wolfe  
Charles Russell

#### HWC ENGINEERING

Scott Wilkinson  
Kevin Miller

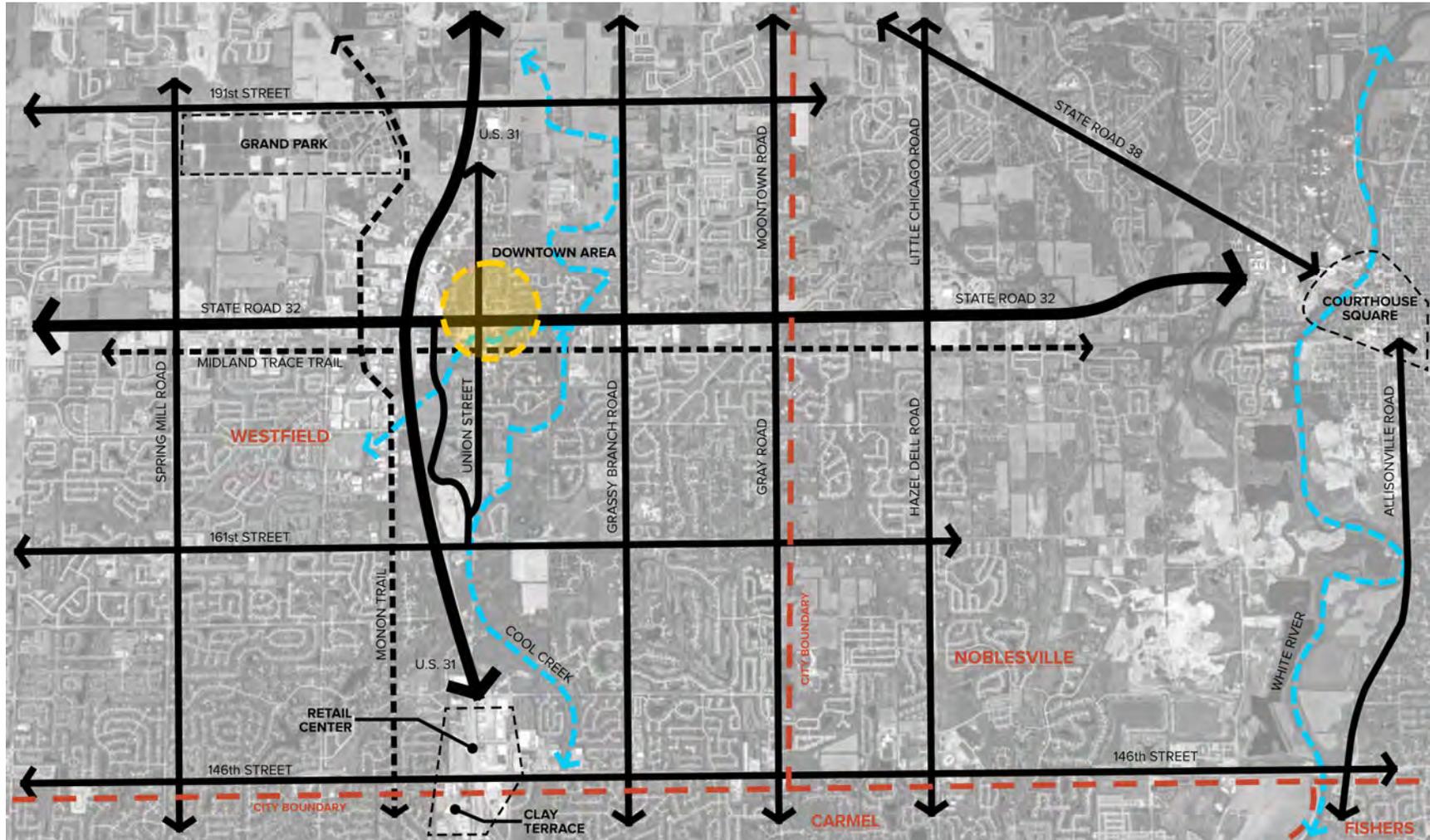
#### SKENDER

Brian Simons

#### PARTNERS

Jeremy Lollar, Old Town Companies  
Westfield-Washington Historical Society  
Westfield Police Department  
Rundell Ernstberger Associates



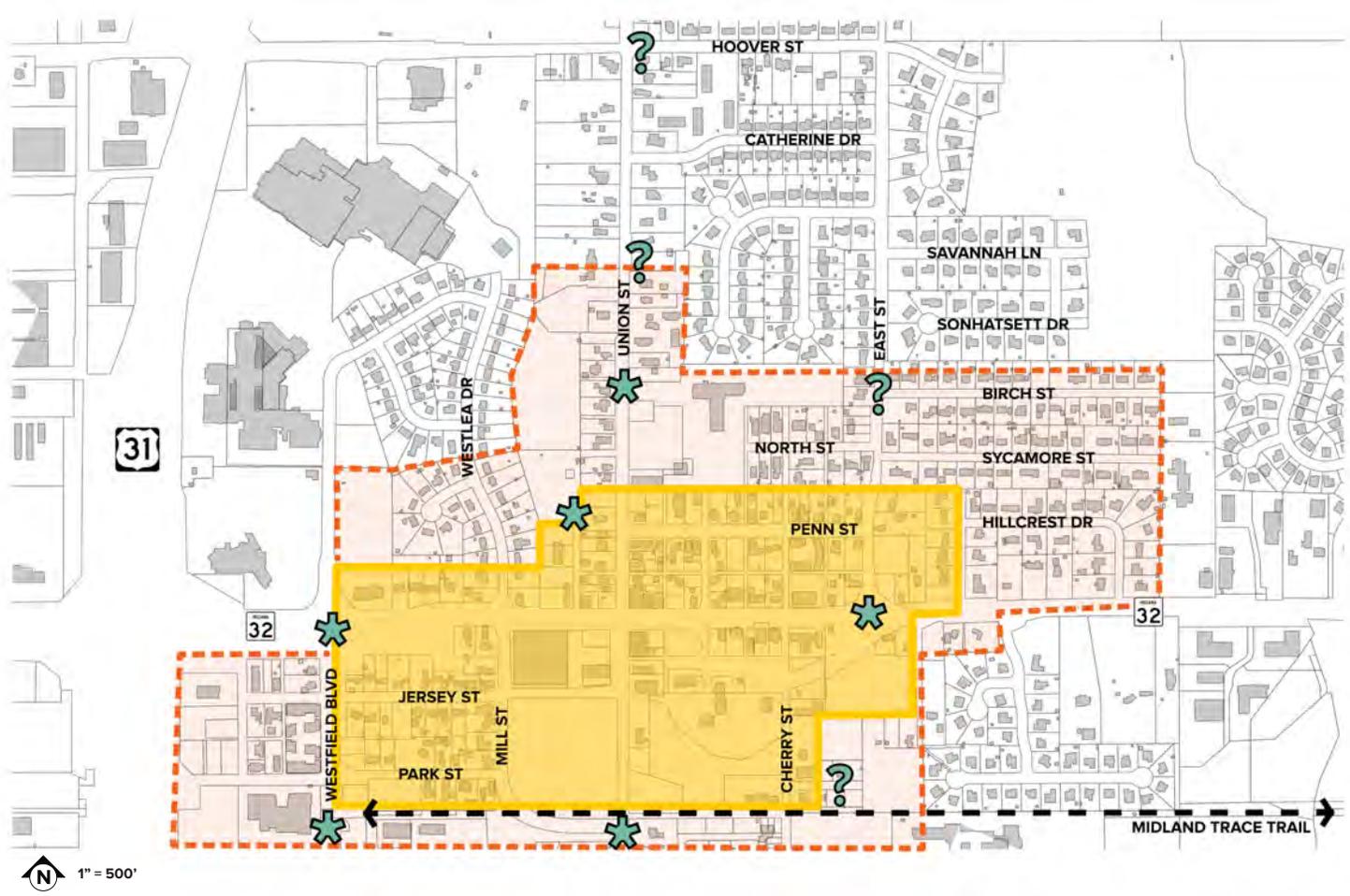


NOT TO SCALE



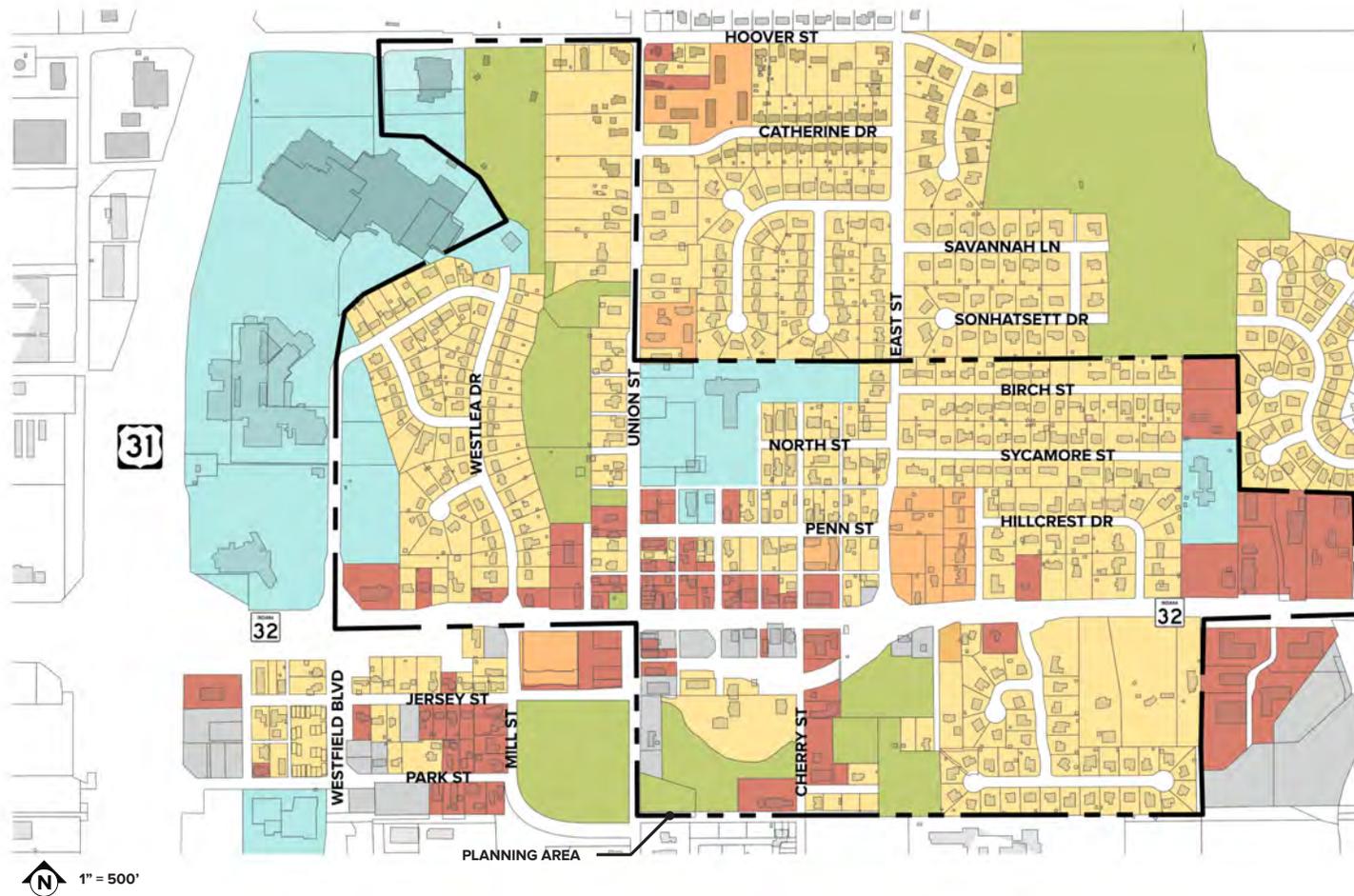
**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | LOCATION & REGIONAL CONTEXT**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 5



- DOWNTOWN CORE**  
 Area of prime downtown development and activity. Dense, with mix of uses.
- DOWNTOWN ADJACENT**  
 Supports downtown core. Less dense, less mix of uses. Residential uses of lower density. Areas of opportunity for core growth in the future.
- WELCOME POINTS**  
 "Moment of Arrival" to downtown, based on look and feel of existing area. Typically indicated by some form of landmark such as the roundabout at SR 32 & Westfield Blvd. or Christ United Methodist Church on N. Union Street.
- POTENTIAL WELCOME POINTS**  
 These may be considered Welcome Points, but either have no definite landmark or are not within a close proximity to the downtown core.
- MIDLAND TRACE TRAIL**

**NOTE:** The boundaries shown are based on initial look and feel of existing downtown character. Areas shown are subject to change throughout planning process.



- SINGLE-FAMILY RESIDENTIAL
- MULTI-FAMILY RESIDENTIAL
- RETAIL/OFFICE
- PUBLIC/INSTITUTIONAL
- PARK/NATURAL AREA
- VACANT

NOTE: Land Uses as of January 21, 2025, subject to change.

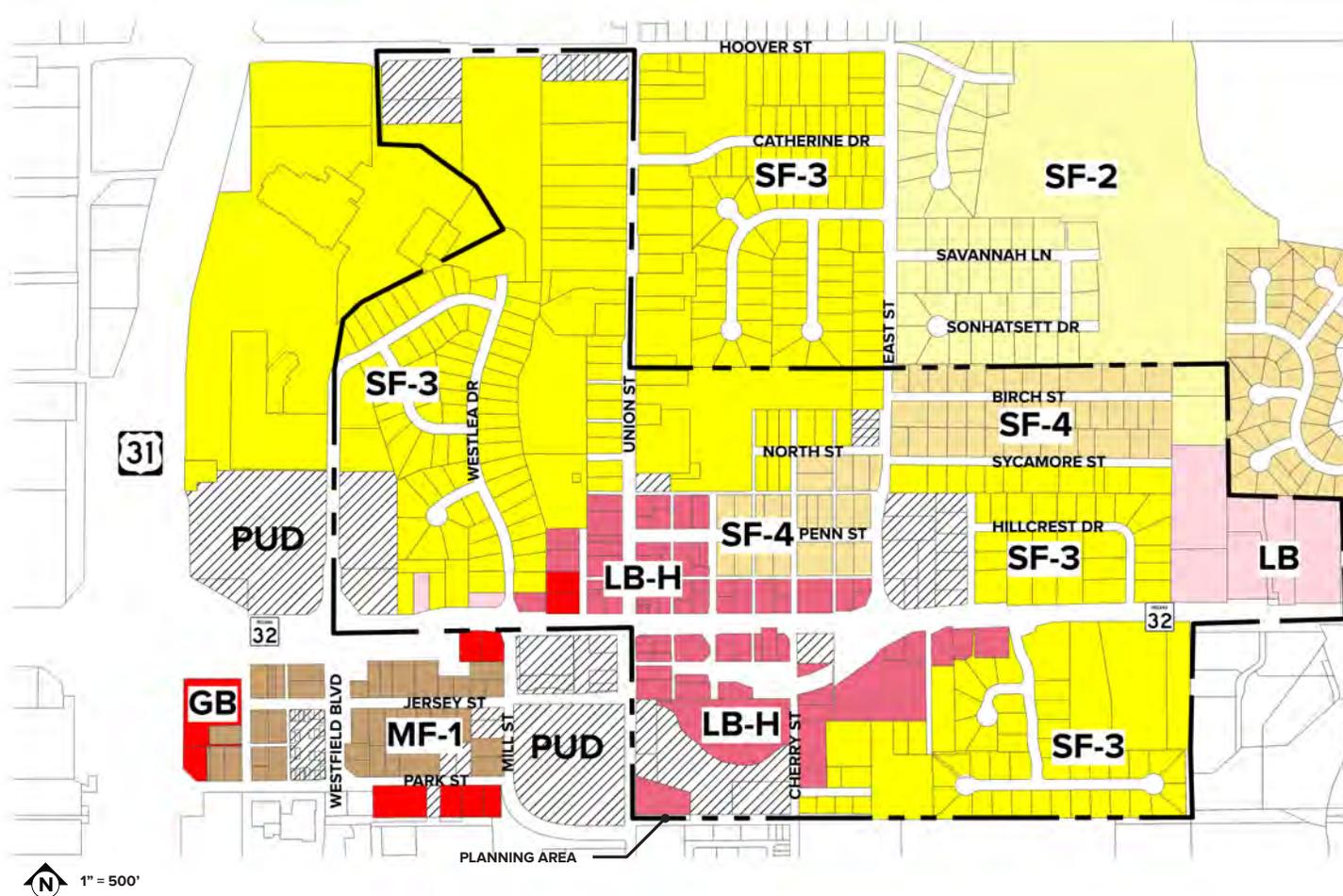
**ANALYSIS**

- Downtown has a mix of uses, including vacant parcels which present an opportunity for infill development.
- Existing open spaces lack sufficient connectivity to each other and the downtown area.
- Asa Bales Park has a pinch point at the end of Westlea Drive. Future redevelopment of the neighborhood could allow for the expansion of the park.



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | EXISTING LAND USES**

© 2025 Browning Day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 7



- SF-2**  
Single-family Low Density
- SF-3**  
Single-family Medium Density
- SF-4**  
Single-family High Density
- MF-1**  
Multi-family Low Density
- LB-H**  
Local Business/Historical District
- LB**  
Local & Neighborhood Business
- GB**  
General Business
- PUD**  
Planned Unit Development

**NOTE:** Zoning Designations, Colors, and Patterns per City of Westfield Zoning Map, as of January 21, 2025. Subject to change.

**ANALYSIS**

- New development has primarily occurred with the use of a Planned Unit Development (PUD), creating varying site and architectural standards that can be difficult to enforce and administrate. 13 different PUDs are within the downtown vicinity.
- At least half a dozen homes along Penn Street have been re-purposed for business use.
- New businesses in the LB-H district often appear residential in character. This is in line with the intent of the LB-H district.
- An overlay is one strategy to administer site and architectural standards in the downtown area.



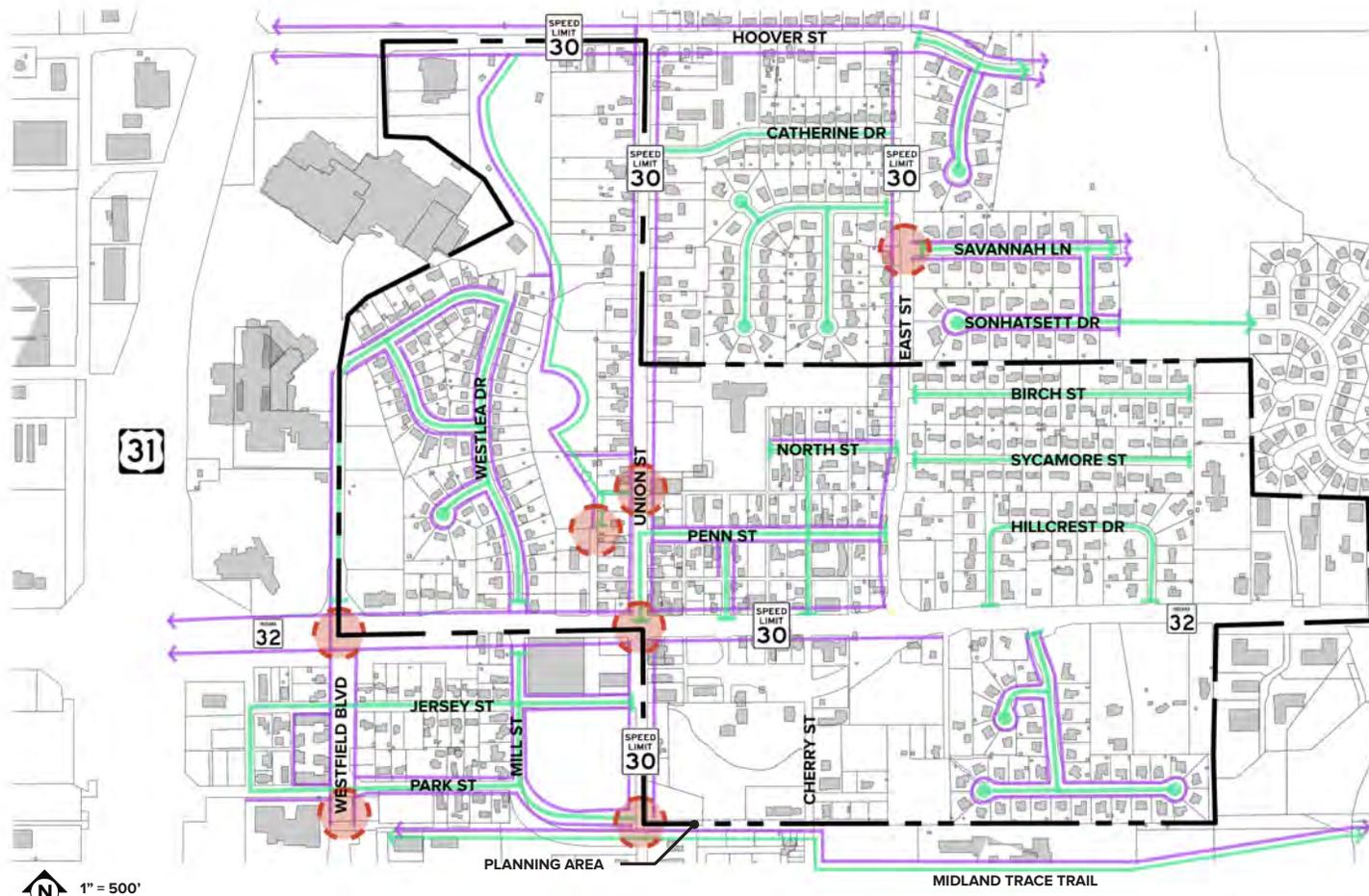
**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | EXISTING ZONING**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 8



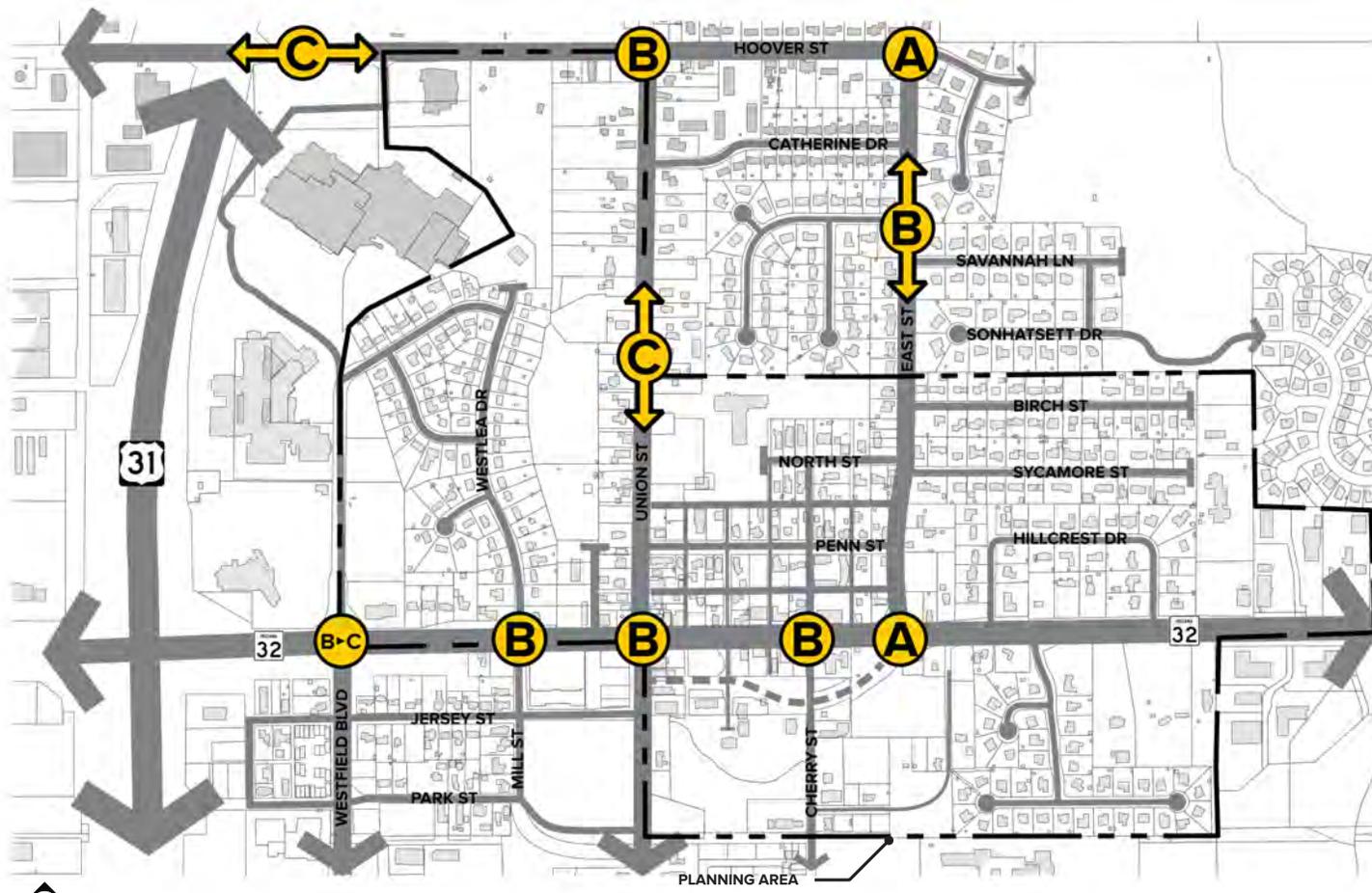
- LISTED**  
On the National Register of Historic Places.
  - OUTSTANDING**  
Has sufficient significance to be listed or is eligible to be listed on the National Register.
  - NOTABLE**  
Above average significance. May be eligible for National Register.
  - CONTRIBUTING**  
Property is important to historic fabric of an area. Must be at least 40 years old and can only be listed in the National Register as part of a District.
  - NATIONAL REGISTER HISTORIC DISTRICT**
- NOTE:** Historic Designations and District Boundary illustrated on this map are defined by National Register of Historic Places, as of January 21, 2025.

- ANALYSIS**
- The Historic District is primarily limited to the intersection of Union Street and SR 32, and buildings on the west side of Union.
  - Outstanding and notable structures are sparse, and not all are located within the designated district.
  - The District primarily draws its significance from contributing structures.
  - Though contributing structures are not individually significant, as a group they contribute to the historic character of downtown.
  - Given the low number of contributing structures, any development that removes these could endanger the historic character of downtown.



-  **SIDEWALKS**
-  **GOLF CART ACCESSIBLE**  
Streets with posted speed limit of 25 MPH or less.
-  **CONFLICT POINTS**  
Areas where vehicles and pedestrians have increased risk of physical conflict due to lack of infrastructure. These indicate areas where infrastructure may need improved.

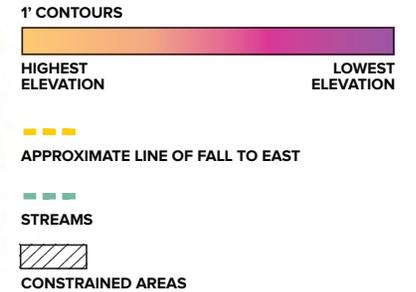
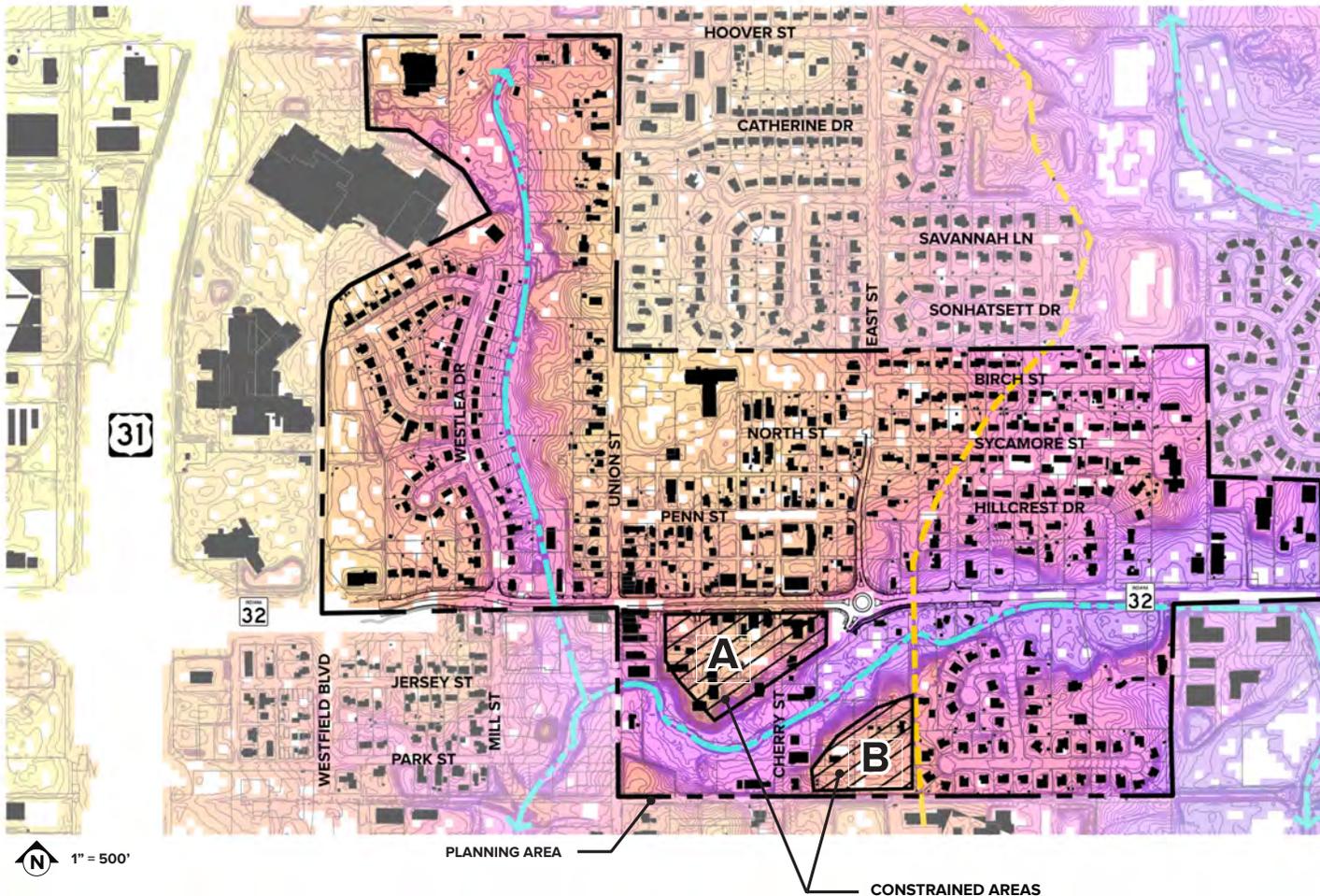
- ANALYSIS**
- Golf carts are permitted on streets with a posted speed limit of 25 MPH or less. Cart users have limited N/S connectivity due to 30 MPH speed limits on Hoover St, Union St, East St, and Westfield Blvd.
  - Golf carts can be used on paths in Asa Bales Park and Midland Trace Trail, but lack crossings/connections at Union Street and Camilla Ct.
  - Pedestrian connectivity is present on major streets but lacks on others.
  - Trail connections are limited.
  - Asa Bales Park has limited connectivity to downtown.



- ROADWAY LEVEL OF SERVICE (EXISTING & FUTURE)**
- A = Free flowing, uninterrupted travel
  - B = Stable flow, low restriction on maneuverability/speed
  - C = Stable flow, maneuverability/speed affected by other vehicles
  - D = High density free flow, restricted maneuverability/speed
- INTERSECTION LEVEL OF SERVICE (EXISTING & FUTURE)**
- A = <10 sec. delay
  - B = 10-20 sec. delay
  - C = 20-35 sec. delay
  - D = 35-55 sec. Delay (Limit of Acceptable Delay)
- INTERSECTION LEVEL OF SERVICE (EXISTING TO FUTURE)**
- B-C

- ANALYSIS**
- Note: Level of Service provided by Road Impact Fee Zone Improvement Plan
  - Study was conducted in 2022 & 2023, while school was in session, and during good weather.
  - Study accounted for improvement of SR 32 and Jersey Street Extension
  - Study did not account for new development within the downtown area.
  - Union Street serves as the primary connection to Westfield schools from US 31 and SR 32
  - Level of Service may decrease depending on types and amount of development that occurs in the downtown area in the future.

1" = 500'



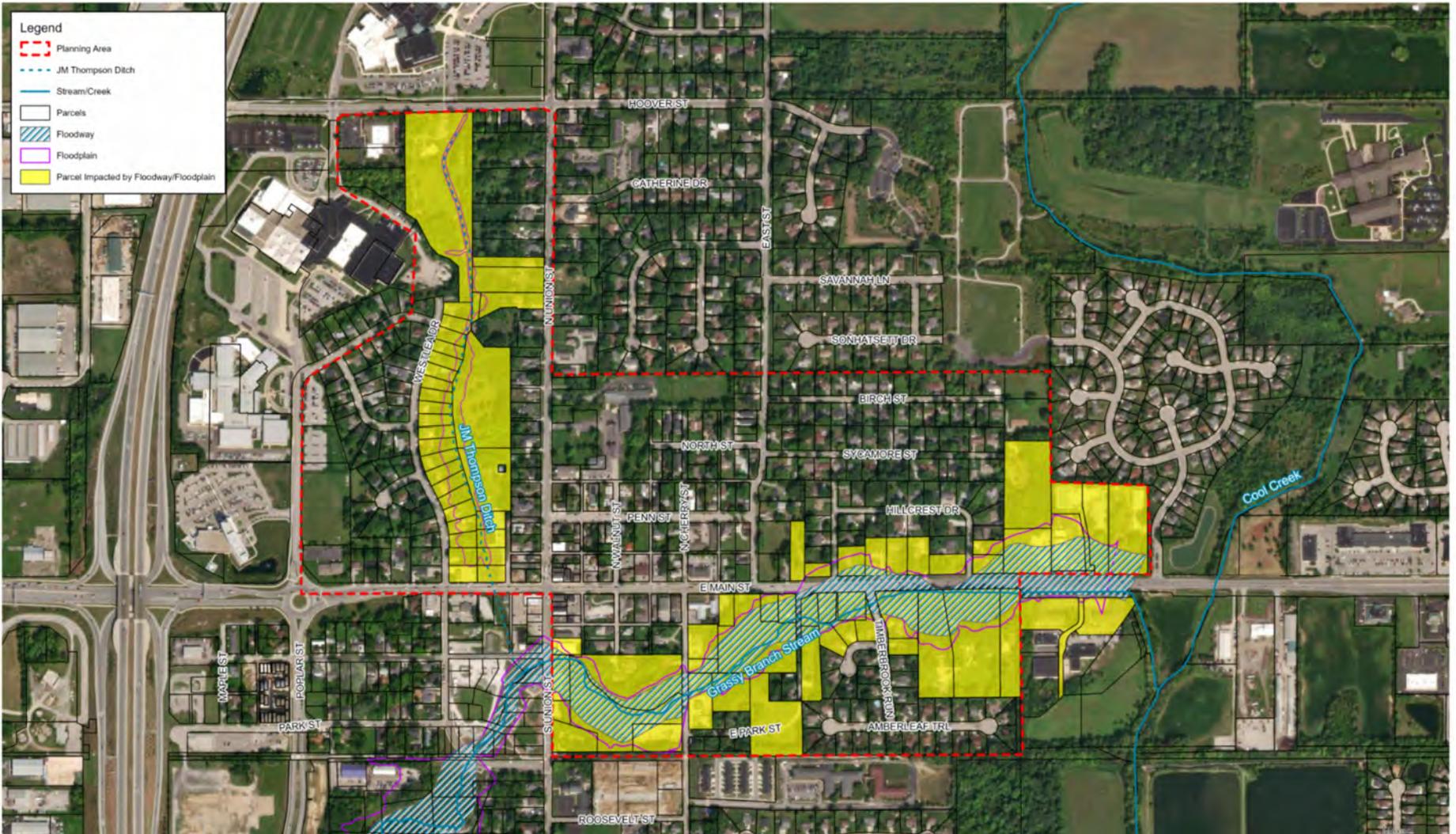
**ANALYSIS**

- The downtown area is significantly affected by Grassy Branch Creek. Though it is a topographical challenge in some areas, it presents an opportunity to present it as a unique feature.
- The eastern edge of downtown drops in elevation towards Cool Creek, the line of fall highlights this.
- The stream extending north towards Hoover St may present a challenge to future development, as land around it is encumbered by floodplain.
- Area A is a constrained portion of land with SR 32 creating a hard boundary to the north, and the topography of Grassy Branch creating a boundary to the south.
- Area B is similarly constrained by Grassy Branch to the north/west, Midland Trace Trail to the south, and residential to the east.



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | NATURAL FEATURES**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 12

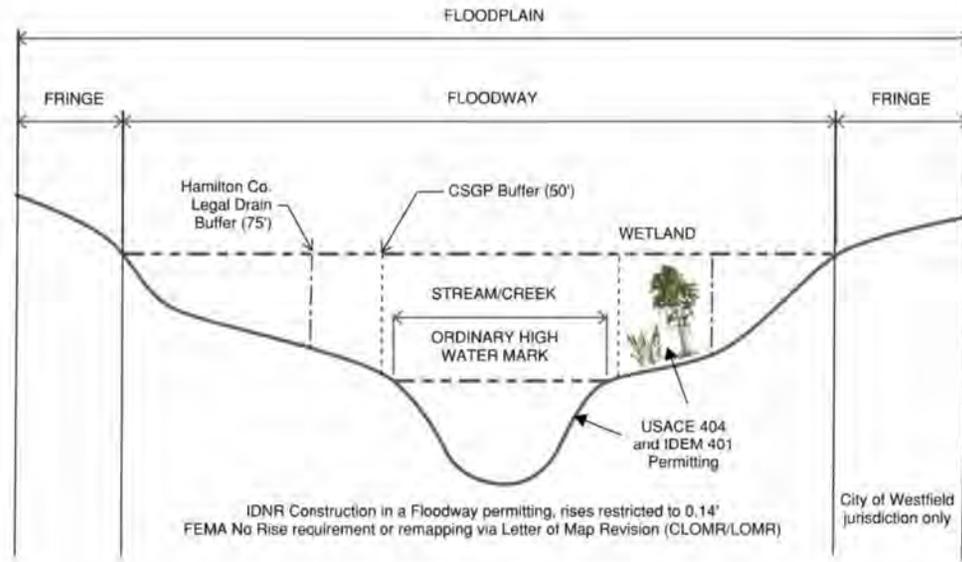


NOT TO SCALE



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | FLOODPLAIN & PARCELS IMPACTED**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 13



Environmental Feature Zone	Controlling Agency	Permitting and Requirements	Potential Mitigation	Anticipated Review Period	Flexibility
JM Thompson Floodplain	IDNR	Floodplain is entirely floodway. IDNR Construction in a Floodway Permit, allowing 0.14' of rise or less within a flood zone. Both modeling and non-modeling submissions are possible within this area.	NA	6 Months	A 0.14' rise is the maximum allowed at your property line, so on property rises can be greater than 0.14'. Non Modeling submissions can be performed for minimal disturbances.
Grassy Branch (Anna Kendall) Flood Fringe	Westfield	Local Improvement Permit. Structures to and utilities must be elevated to 2' above the 100-Yr Water Surface elevation. Structures can be elevated on fill or on part of a structure that can allow flow to pass through like a parking facility or storage area.	1:1 Compensatory Storage requirement.	3 Months	For every cubic yard filled within the flood fringe, a cubic yard must be cut within the flood fringe as well. A common area for cut might offer a sort of "bank" for others to use for fill.
Grassy Branch (Anna Kendall) Floodway	IDNR and FEMA	IDNR Construction in a Floodway Permit, allowing 0.14' of rise or less within a flood zone. Non Modeling and Modeling submissions are possible. Modeling submissions and mitigation design require more expensive design. FEMA permits No Rise (0.00') in this zone unless a Conditional Letter of Map Revision process is approved. This review time is 18 months and is subsequent to the IDNR Review. It will remap the flood zone permanently. A subsequent Letter of Map Revision with As Built info would also be required.	2:1 to 4:1 habitat replacement for wooded or wetland areas (\$100,000/ac)	6-24 Months	A 0.14' rise is the maximum allowed at your property line, so on property rises can be greater than 0.14'. Typically projects in this zone are going for 0.00' rise to avoid the lengthy review time.
Wetland and In-Stream (below the Ordinary High Water Mark) Disturbance	IDEM and USACE	IDEM 401 Water Quality Certification and USACE 404 Dredge and Fill Permit. Nationwide Permits allow for certain activities; Regional General Permit allows for 300' of instream disturbance, 0.1 acre of wetland or water disturbance, or 150' of encasulation; Individual permits require mitigation but can allow impacts greater than Regional General Permit (RGP). Individual Permitting and Mitigation require more expensive designs.	2:1 to 4:1 habitat replacement for wetland area disturbance greater than one acre (\$100,000/Ac), 1:1 stream mitigation (\$500/ft)	3-6 Months	Nationwide Permits can allow for most activities proposed within this zone. The National Wetlands Inventory map does not indicate that wetlands are likely within this zone. Impacts to the stream are likely above Regional General Permit.
Site Disturbance Over 1 Ac	IDEM	Construction Stormwater General Permit. Review for projects will be performed by Westfield MSA except for City owned projects which are reviewed by Hamilton Co SWCD. This permit requires a buffer of up to 50' to be maintained from the Ordinary High Water Mark of the stream, which is where water normally flows within the stream.	NA	2 Months	Encroachment into this zone is not typically allowed. Projects <1Ac do not have the stream buffer requirement. Foot trails/people paths whether they be impervious or pervious in nature may be established within the buffer area with minimal impact to the buffer. Bridges and water dependent structures like boat ramps are exempt from the Buffer Requirement.
Legal Drain	Hamilton County	Legal Drain Encroachment permit. Generally Hamilton Co protects their 75' easement from top of bank.	NA	3 Months	Encroachment into the legal drain zone should be negotiated with the County Surveyor. They have complete authority over their drain and can modify the drain to meet the needs of their community.

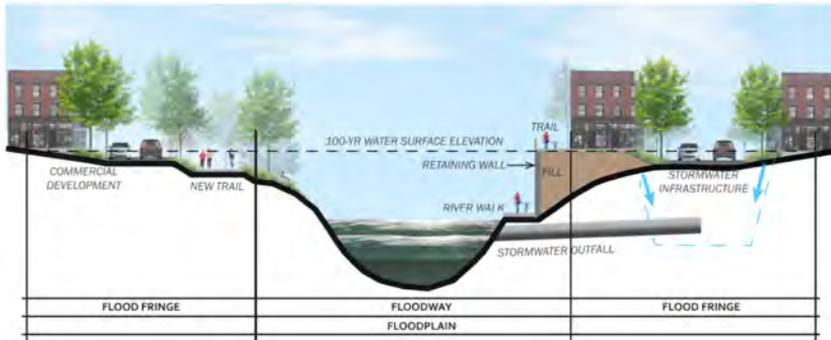


## WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | ENVIRONMENTAL FEATURES

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 14

## HIGH IMPACT CORRIDOR

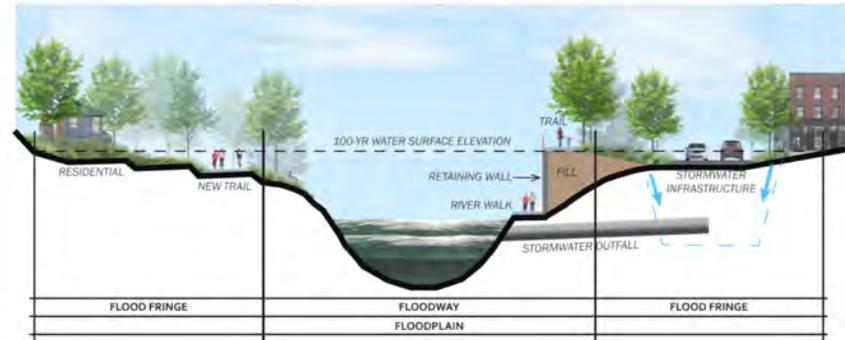
GRASSY BRANCH



Environmental Feature Zone	Permitting and Requirements	Potential Mitigation	Anticipated Review Period
Grassy Branch (Anna Kendall) Flood Fringe	Structure, utility and walk elevation necessary with fill, structure elevation, or stilts. Fill requires compensatory storage. Structure Elevation requires passing flow through the structure on the bottom level, like a parking garage.	1:1 Compensatory Storage requirement.	3 Months
Grassy Branch (Anna Kendall) Floodway	Modeling submissions would be necessary for encroachment on the floodway making design more expensive. CLOMR/LOMR process likely due to impacts, creating lengthy review time. Mitigation is likely due to impacts to wooded corridors.	2:1 to 4:1 habitat replacement for wooded or wetland areas (\$100,000/ac)	24 Months
Wetland and In-Stream (below the Ordinary High Water Mark) Disturbance	Due to the length and type of impact, this zone is likely to need individual permitting with mitigation, making design more expensive and review time more lengthy.	2:1 to 4:1 habitat replacement for wetland area disturbance greater than one acre (\$100,000/Ac); 1:1 stream mitigation (\$500/ft)	6 Months
Site Disturbance Over 1 Ac	Impacts for sites may or may not be over 1 acre.	NA	2 Months
Legal Drain	Hamilton County would need to agree to any encroachment within their 75' Easement	NA	3 Months

## MEDIUM IMPACT CORRIDOR

GRASSY BRANCH



Environmental Feature Zone	Permitting and Requirements	Potential Mitigation	Anticipated Review Period
Grassy Branch (Anna Kendall) Flood Fringe	Structure, utility and walk elevation necessary with fill, structure elevation, or stilts. Fill requires compensatory storage. Structure Elevation requires passing flow through the structure on the bottom level, like a parking garage.	1:1 Compensatory Storage requirement.	3 Months
Grassy Branch (Anna Kendall) Floodway	Modeling submissions would be necessary for encroachment on the floodway making design more expensive. CLOMR/LOMR process could be avoided if rises are kept at 0.00'. Mitigation can be avoided by avoiding tree clearing.	To be Avoided	6 Months
Wetland and In-Stream (below the Ordinary High Water Mark) Disturbance	Minimal impacts within the RGP allowance will avoid mitigation, make design less expensive, and lower review times.	To be Avoided	3 Months
Site Disturbance Over 1 Ac	Impacts for sites may or may not be over 1 acre.	NA	2 Months
Legal Drain	Hamilton County would need to agree to any encroachment within their 75' Easement	NA	3 Months

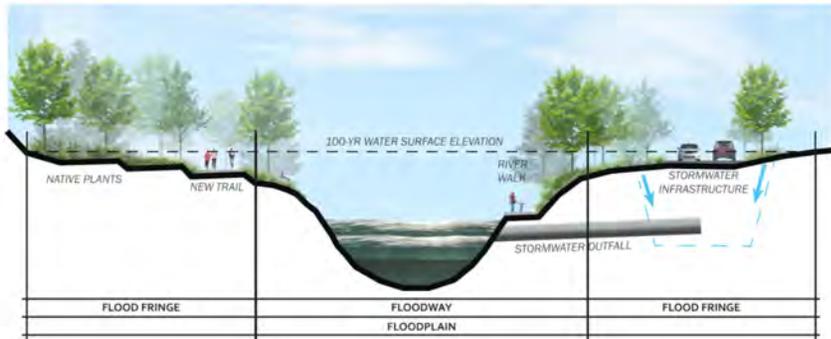


## WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | FLOODPLAIN - POTENTIAL IMPACTS

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 15

## LOW IMPACT CORRIDOR

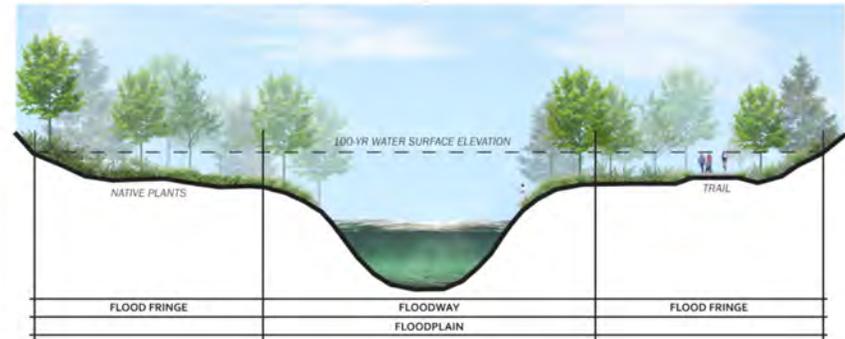
GRASSY BRANCH



Environmental Feature Zone	Permitting and Requirements	Potential Mitigation	Anticipated Review Period
Grassy Branch (Anna Kendall) Flood Fringe	Avoiding changes in grade within the flood zone can allow for mitigation to be avoided	To be Avoided	3 Months
Grassy Branch (Anna Kendall) Floodway	Non-modeling submissions can be completed for projects that do not propose change in grades. Mitigation can be avoided by avoiding tree clearing.	To be Avoided	6 Months
Wetland and In-Stream (below the Ordinary High Water Mark) Disturbance	Minimal impacts within the RGP allowance will avoid mitigation, make design less expensive, and lower review times.	To be Avoided	3 Months
Site Disturbance Over 1 Ac	Impacts for sites may or may not be over 1 acre.	NA	2 Months
Legal Drain	Hamilton County would need to agree to any encroachment within their 75' Easement	NA	3 Months

## FLOODPLAIN CORRIDOR

JM THOMPSON DITCH

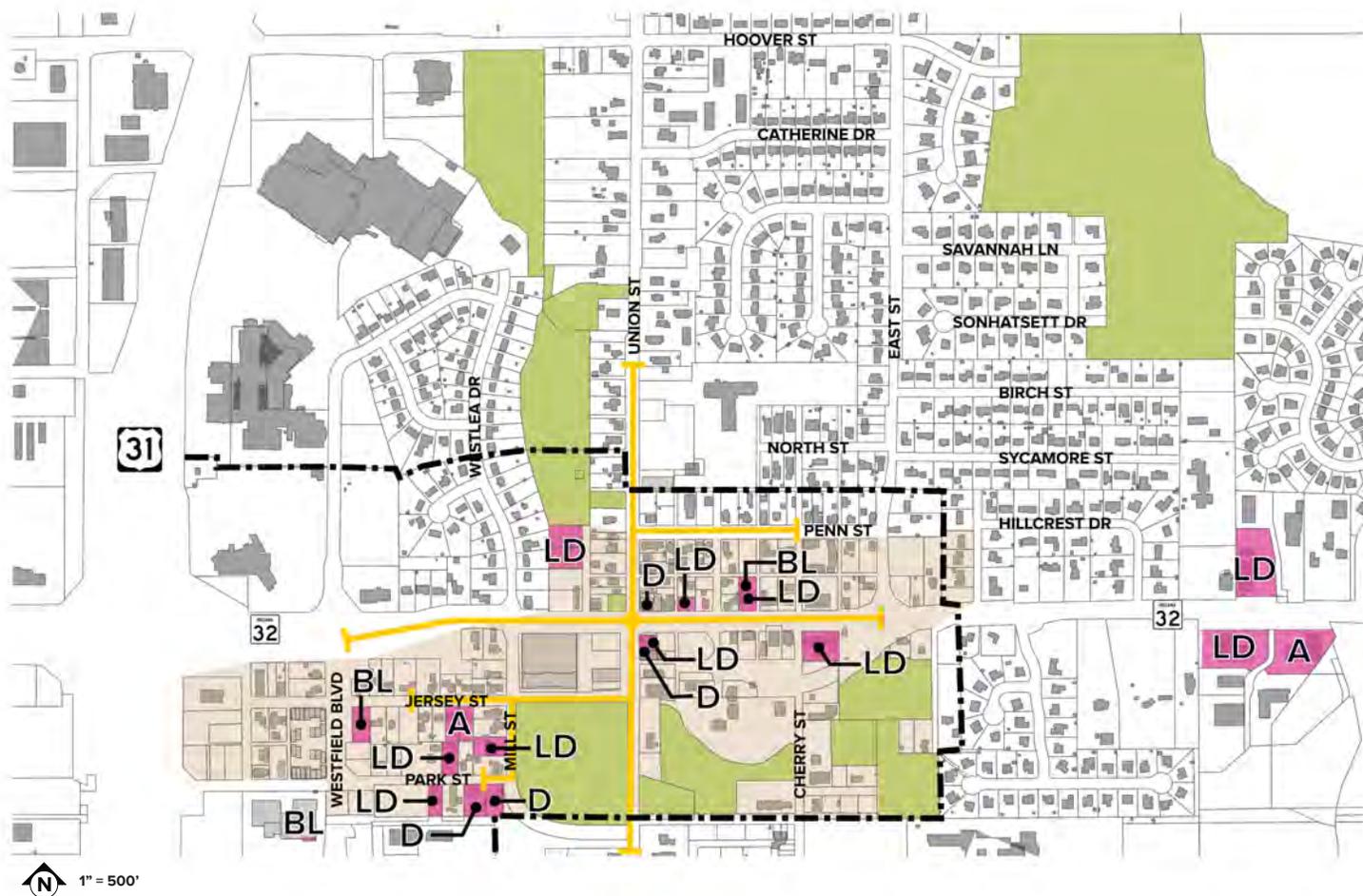


Environmental Feature Zone	Permitting and Requirements	Potential Mitigation	Anticipated Review Period
JM Thompson Floodplain	Non-modeling submissions can be completed for projects that do not propose change in grades. Mitigation can be avoided by avoiding tree clearing.	To be Avoided	6 Months
Wetland and In-Stream (below the Ordinary High Water Mark) Disturbance	Minimal impacts within the RGP allowance will avoid mitigation, make design less expensive, and lower review times.	To be Avoided	3 Months
Site Disturbance Over 1 Ac	Impacts for sites may or may not be over 1 acre.	NA	2 Months
Legal Drain	Hamilton County would need to agree to any encroachment within their 75' Easement	NA	3 Months



## WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | FLOODPLAIN - POTENTIAL IMPACTS

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 16



- RETAIL/OFFICE
- PARK/NATURAL AREA
- FOOD/BEVERAGE ESTABLISHMENTS
- DESIGNATED OUTDOOR REFRESHMENT AREA
- RIVERFRONT DISTRICT
- 5 MINUTE WALKING DISTANCE FROM UNION/32

- OPERATING TIMES**
- BL** - BREAKFAST - LUNCH
  - LD** - LUNCH - DINNER
  - D** - DINNER ONLY
  - A** - ALL DAY

- ANALYSIS**
- The existing dining options primarily serve the lunch and dinner timeframe, with a gap in morning options.
  - The bulk of establishments are within a 5 minute walk of the Union/SR 32 intersection, creating a density that allows patrons to walk from one establishment to another.
  - Existing establishments would benefit from residential units in the downtown area.

**ANALYSIS**

The following study and findings compares Westfield’s downtown area to other nearby cities or those with similar features.

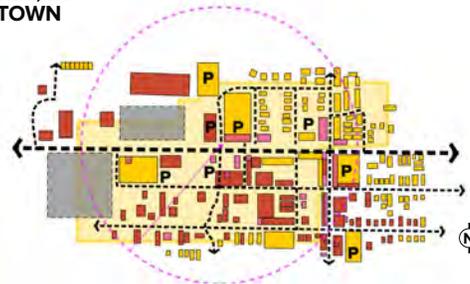
- Downtown has a mix of uses that can provide enough use diversity to ensure the downtown is not boxed into a single identity in the future.
- Asa Bales Park and Grand Junction Park are unique features of Westfield’s downtown.
- Westfield’s downtown is threatened by SR 32 potentially cutting the downtown area in half.
- The downtown area has promising pedestrian circulation, but will need to fill in a few gaps.
- Grassy branch Creek presents an opportunity for downtown to have a unique character.
- Existing and future planned parking garages are primarily located in the southwestern quadrant. The lack of parking in the other quadrants may create parking challenges in the future, if not addressed with a comprehensive parking strategy in the redevelopment plan.



**FISHERS, IN**



**CARMEL, IN  
MIDTOWN**



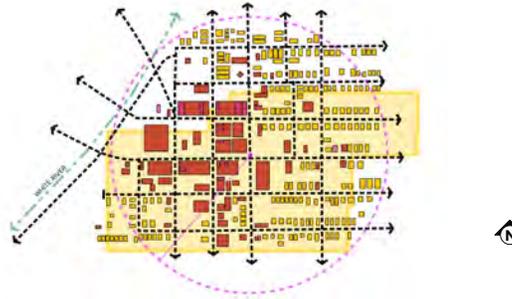
**NASHVILLE, IN**



**NAPERVILLE, IL**



**NOBLESVILLE, IN**

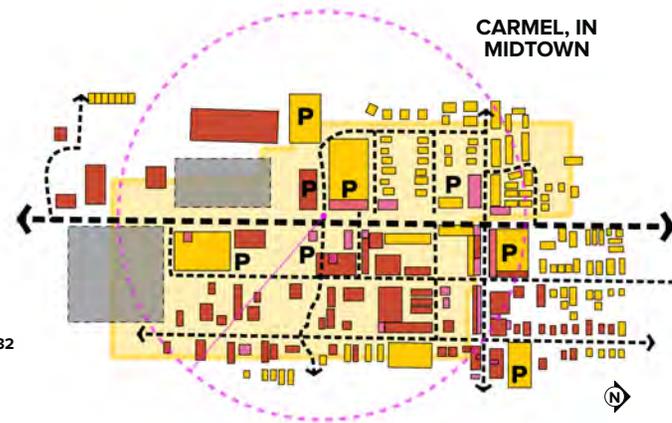
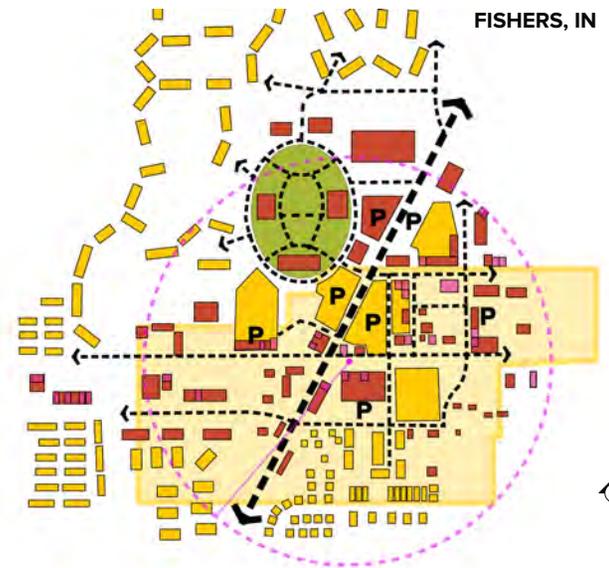


**ZIONSVILLE, IN**



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | SCALE STUDIES SUMMARY**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 18



NOT TO SCALE

**SCALE STUDIES SUMMARY**

**FISHERS**

Organized along two axes, 116th St and Nickel Plate Trail, Fishers has created density and activity around the intersection of these two axes. Parking is primarily provided by multiple garages.

**CARMEL MIDTOWN**

Organized around the Monon Trail, Midtown has a commercial core with residential uses surrounding it. Parking garages are spread evenly to support the core of the district.

- RESIDENTIAL
- RETAIL/OFFICE
- FOOD & DRINK
- GREEN SPACE
- 5 MINUTE WALKING DISTANCE FROM UNION/32
- PEDESTRIAN PATHS
- P** PARKING GARAGE



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | SCALE STUDIES**

© 2025 Browning Day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 19



**SCALE STUDIES SUMMARY**

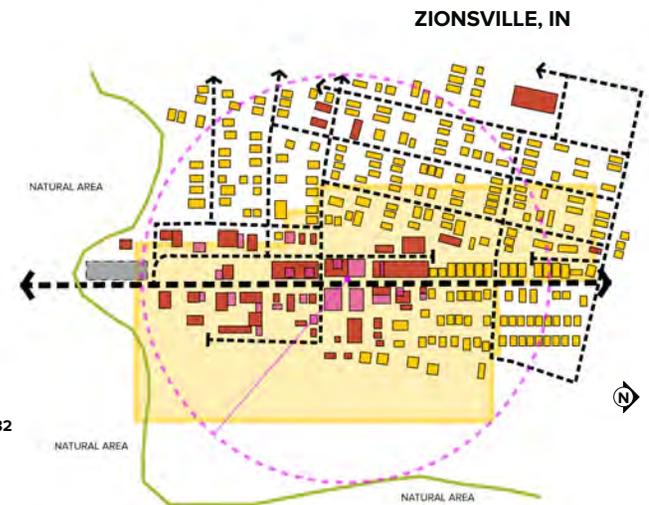
**NASHVILLE**

Located on a state highway, Nashville is primarily a tourist destination. With most of the retail uses on the main street, Nashville utilizes small surface lots and streets for circulation/parking.

**ZIONSVILLE**

A traditional main street, relying on street parking to serve its patrons. It primarily serves as a shopping street, with an adjacent street providing thru access for the majority of traffic.

- RESIDENTIAL
- RETAIL/OFFICE
- FOOD & DRINK
- GREEN SPACE
- 5 MINUTE WALKING DISTANCE FROM UNION/32
- PEDESTRIAN PATHS
- P** PARKING GARAGE

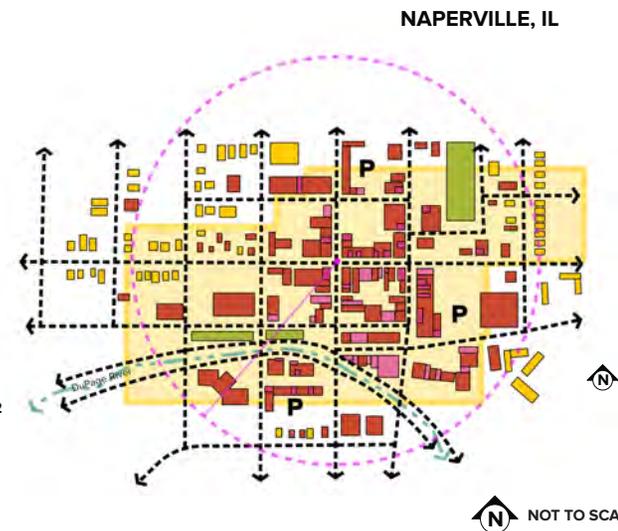


NOT TO SCALE



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | SCALE STUDIES**

© 2025 Browning Day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 20



**SCALE STUDIES SUMMARY**

**NOBLESVILLE**

A traditional town square on a grid of circulation. The grid efficiently organizes uses and circulation.

**NAPERVILLE**

This town capitalizes its location on the DuPage River by orienting uses and circulation along the river walk.

- RESIDENTIAL
- RETAIL/OFFICE
- FOOD & DRINK
- GREEN SPACE
- 5 MINUTE WALKING DISTANCE FROM UNION/32
- PEDESTRIAN PATHS
- P** PARKING GARAGE

NOT TO SCALE



**WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | SCALE STUDIES**

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 21

## ANALYSIS | GAPS AND OPPORTUNITIES

Towards the midway point of the **Discovery and Organization** phase, the design team conducted an internal charrette to help illustrate and define the opportunities and gaps within our understanding of Downtown Westfield. This informal process led to the following **Gap and Opportunity Analysis**.

Each of the four “schemes” shown on the following pages shows an organizing framework for the vision of downtown. Each one highlights key opportunities and the gaps not yet solved. They build on each other; a gap identified in “Scheme A” may also exist in scheme B, but because B is only interested in identifying new gaps and opportunities, all are not explicitly identified each time.

Additionally the concepts explored in each framework are high-level and deliberately unresolved. We are not yet solving problems with development and buildings. Instead to clearly understand and analyze the future of downtown, each framework takes a look towards the future. Think of it like **organizing puzzle pieces by shape** to get an understanding the of larger problem ahead.

### CONCEPTS EXPLORED

This is the organizing framework for the downtown - a mix of development, zoning, concepts, and themes, not intended to be any specific recommendation for development at this time.

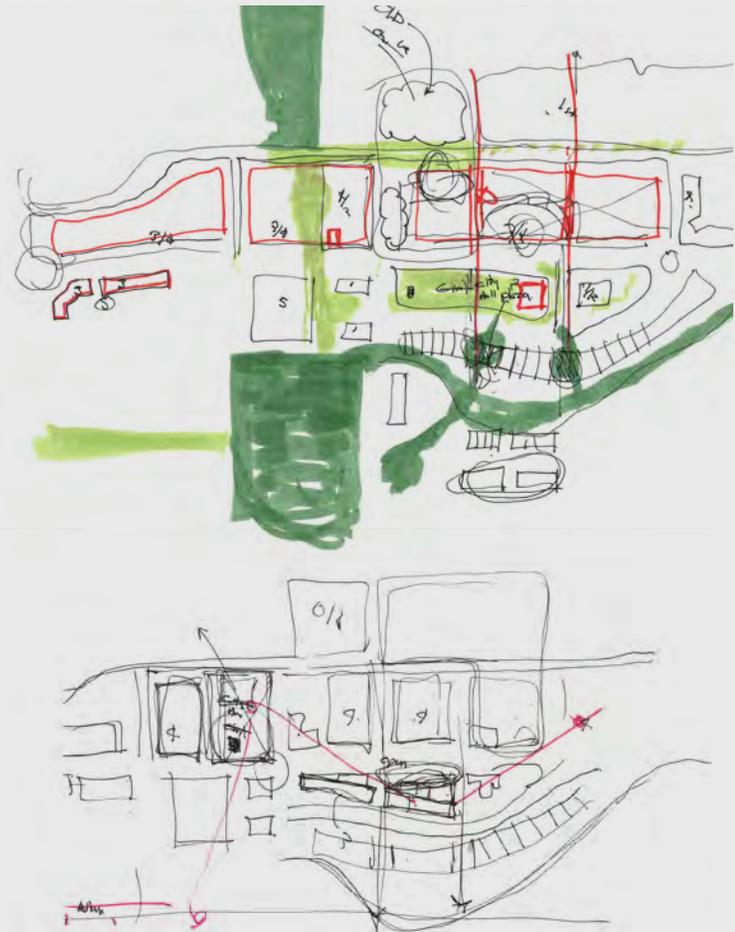
### OPPORTUNITIES REVEALED

These are the benefits of a particular organizing framework. Can be anything from increased circulation patterns to a particular use pattern for a group of parcels. These are indented to reveal areas of focus. Are the same parcels and areas coming up again and again? Those may be areas to zero in on for implementation of catalyst development.

### GAPS EXPOSED

Where does the fabric of downtown fall short? As the frameworks are applied are these issues solved or amplified? This is where the primary problems to be solved through the rest of the vision plan are identified and quantified. The list is not exhaustive, merely designed to surface gaps not otherwise understood clearly in other discovery phase activities.

EXAMPLES OF WORKING SKETCHES AND IDEAS GENERATED DURING THE DESIGN CHARRETTE  
FOR ILLUSTRATION ONLY

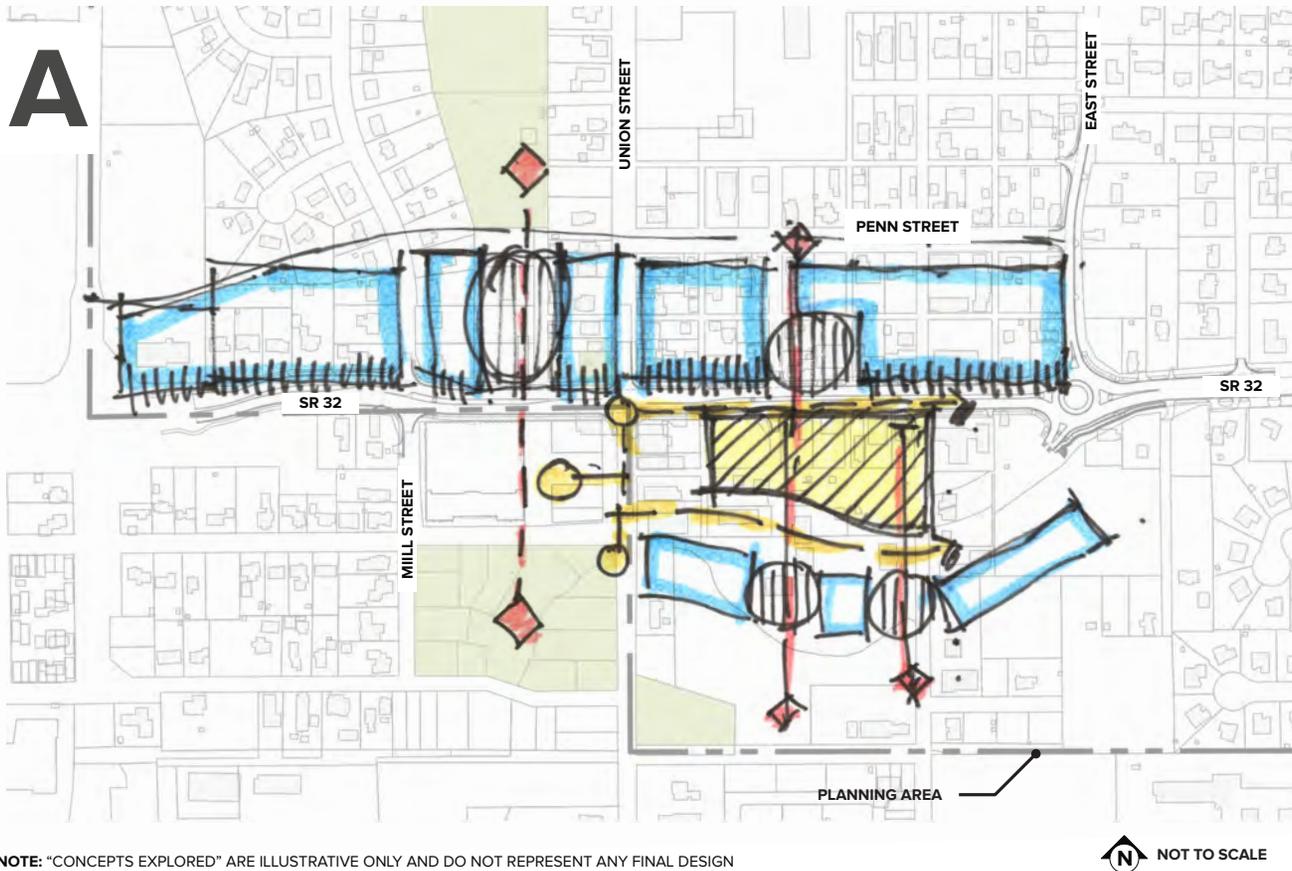


 NOT TO SCALE



## WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | GAP ANALYSIS SUMMARY

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 22



**NOTE:** "CONCEPTS EXPLORED" ARE ILLUSTRATIVE ONLY AND DO NOT REPRESENT ANY FINAL DESIGN IDEAS, BUILDING MASSING, OR POTENTIAL DEVELOPMENTS; THEY ARE MERELY TO FRAME THE OPPORTUNITIES AND GAPS WITHIN THE DOWNTOWN

## A | "URBAN WALL"

### CONCEPTS EXPLORED

- "Wall" of 3-4 story development along north side of SR 32
- "Backstop" of townhouse / single family south of Jersey Street
- City center plaza and municipal spaces located along the parcels between SR 32 and Jersey Street

### OPPORTUNITIES REVEALED

- SR 32/Jersey municipal complex can create an east-west pedestrian connection and urban focal point (open public plaza or similar)
- Taller development along SR 32 emphasizes a downtown feel and mirrors new and planned projects south of SR 32 - gives downtown height and substance (desired? Unclear...)
- "Cuts" in the urban walls will be critical to movement between various types of spaces

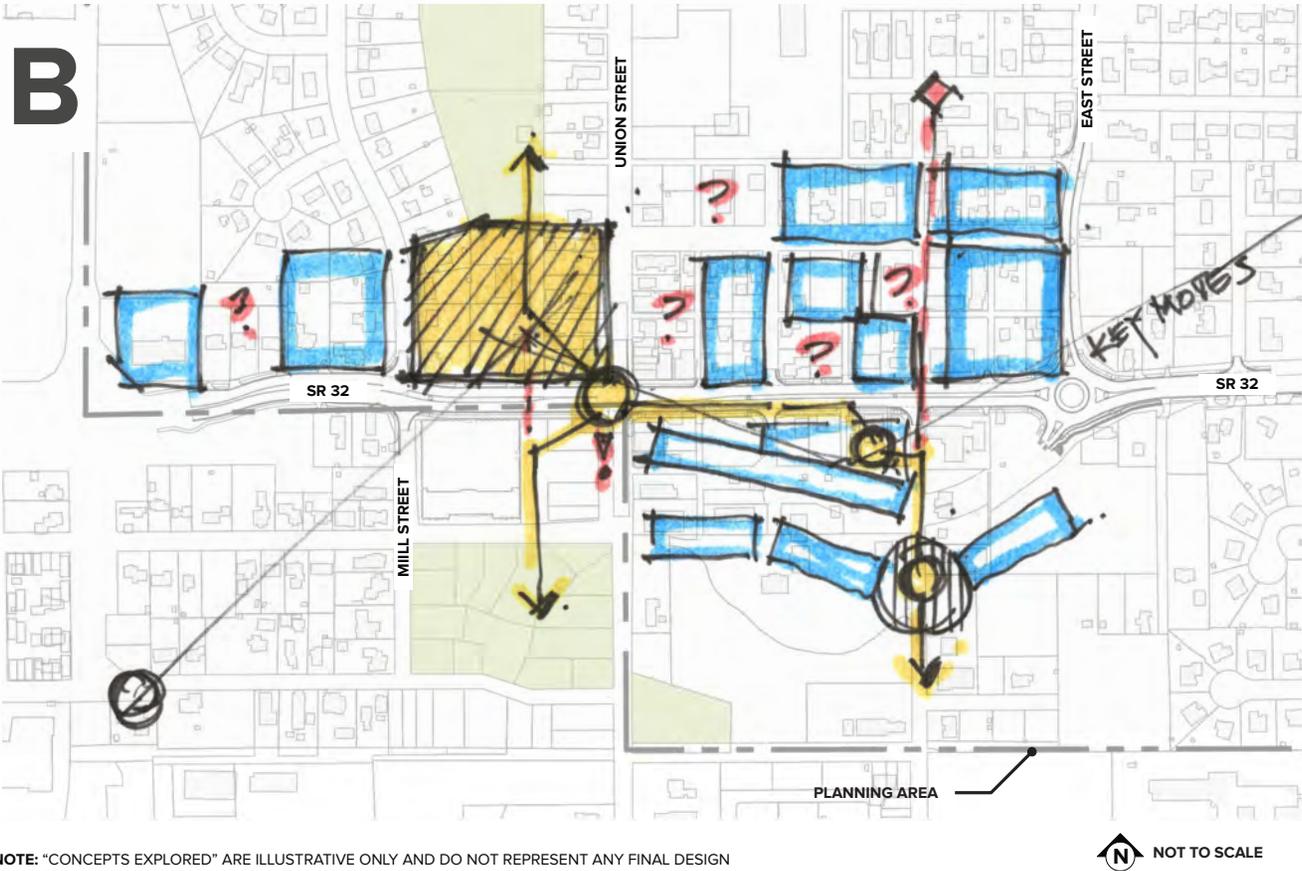
### GAPS EXPOSED

- Urban pedestrian space connections between "here" and "there"; notably
  - Union Square east to parcels between Cool Creek and SR 32
  - North between parks; union square visually connects Grand Junction Park with SR 32 axis - what connects Asa Bales Park to SR 32-axis (distillery has potential with public-private outdoor space, car dealer in the way)
  - Eastern portion of downtown, north to south
  - Connection to creek/greenway becomes secondary between the backstop of single family development
- Parking to the far SE in new townhouse development, how does that tie into east development potential?
- Same as above but for north of SR 32.
- What does the "Welcome to Westfield" feel like for arrival from US 31? From Carey on SR 32?



## WESTFIELD DOWNTOWN REDEVELOPMENT PLAN | GAP ANALYSIS A - URBAN WALL

© 2025 browning day | Westfield Downtown Redevelopment Plan | Project #24029 | January 29, 2024 | 23



**NOTE:** "CONCEPTS EXPLORED" ARE ILLUSTRATIVE ONLY AND DO NOT REPRESENT ANY FINAL DESIGN IDEAS, BUILDING MASSINGS, OR POTENTIAL DEVELOPMENTS; THEY ARE MERELY TO FRAME THE OPPORTUNITIES AND GAPS WITHIN THE DOWNTOWN

**B | "LINKED NODES"**

**CONCEPTS EXPLORED**

- City center plaza and municipal spaces located at Carnegie Library site (NW corner of Union and SR 32)
- Individual 3-5 story development parcels along north side of SR 32 develop over time (similar to size and scale of union square building)
- Angled multifamily along SR 32/Jersey parcels with ground floor retail and open public-private urban spaces shaped by street network

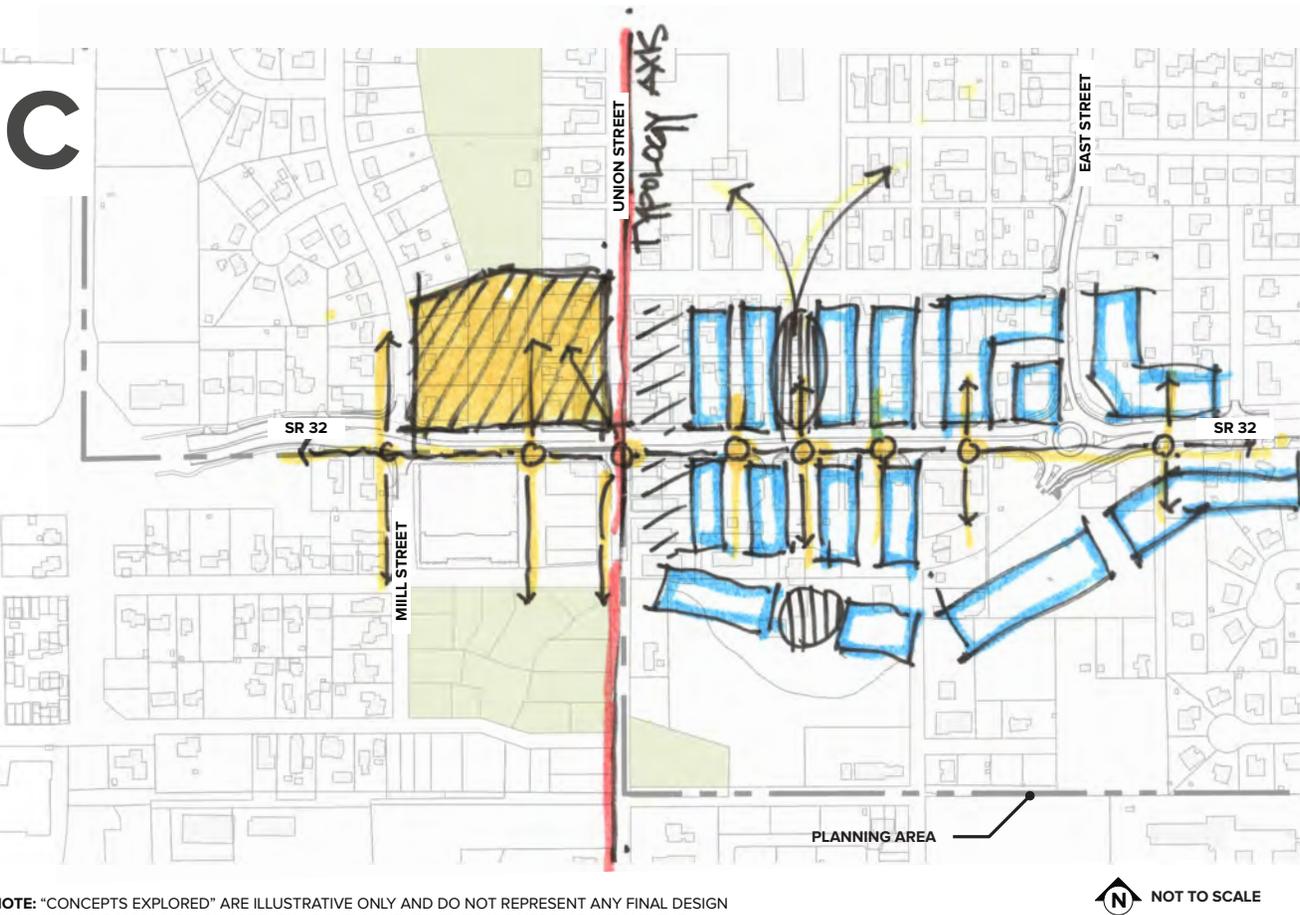
**OPPORTUNITIES REVEALED**

- Integration of new and old creates unique architectural and open space combinations of urban spaces.
- Municipal plaza at NW Union/SR 32 fills a connection need between south side of downtown and north to Asa Bales Park.
- Development of a varied depth plaza/retail/pocket park pedestrian experiences along 32 can active that as a pedestrian movement corridor in addition to vehicles (rewards planned investment in streetscape)
- Parcelled development along SR 32 is easier to plan for and encourage when coupled with robust design guidelines/comp plan; allows more natural development over time as single family phases out

**GAPS EXPOSED**

- How can downtown have a thoughtful identity when development is more piecemeal and over time?
- How does the SR 32/Jersey development park to support that retail investment?
- What does the pedestrian movement look like N-S at Union/SR 32 with the municipal plaza located on the north side and current/short term future housing to the south?
  - Also how does the civic center park?
- What is the incentive for the single-family businesses to "scale up" to full retail development?





C | “TYPOLGY AXIS”

CONCEPTS EXPLORED

- City center plaza and municipal spaces located at Carnegie Library site
- Individual 3-5 story development parcels along north side of SR 32 develop, west of Union (similar to size and scale of union square building) - *not shown, carried over from B*
- Rowhouse, alley loaded developments perpendicular to SR 32, deadheaded with small scale retail and living overtop at SR 32 streetscape (could be live/work or sim.)

OPPORTUNITIES REVEALED

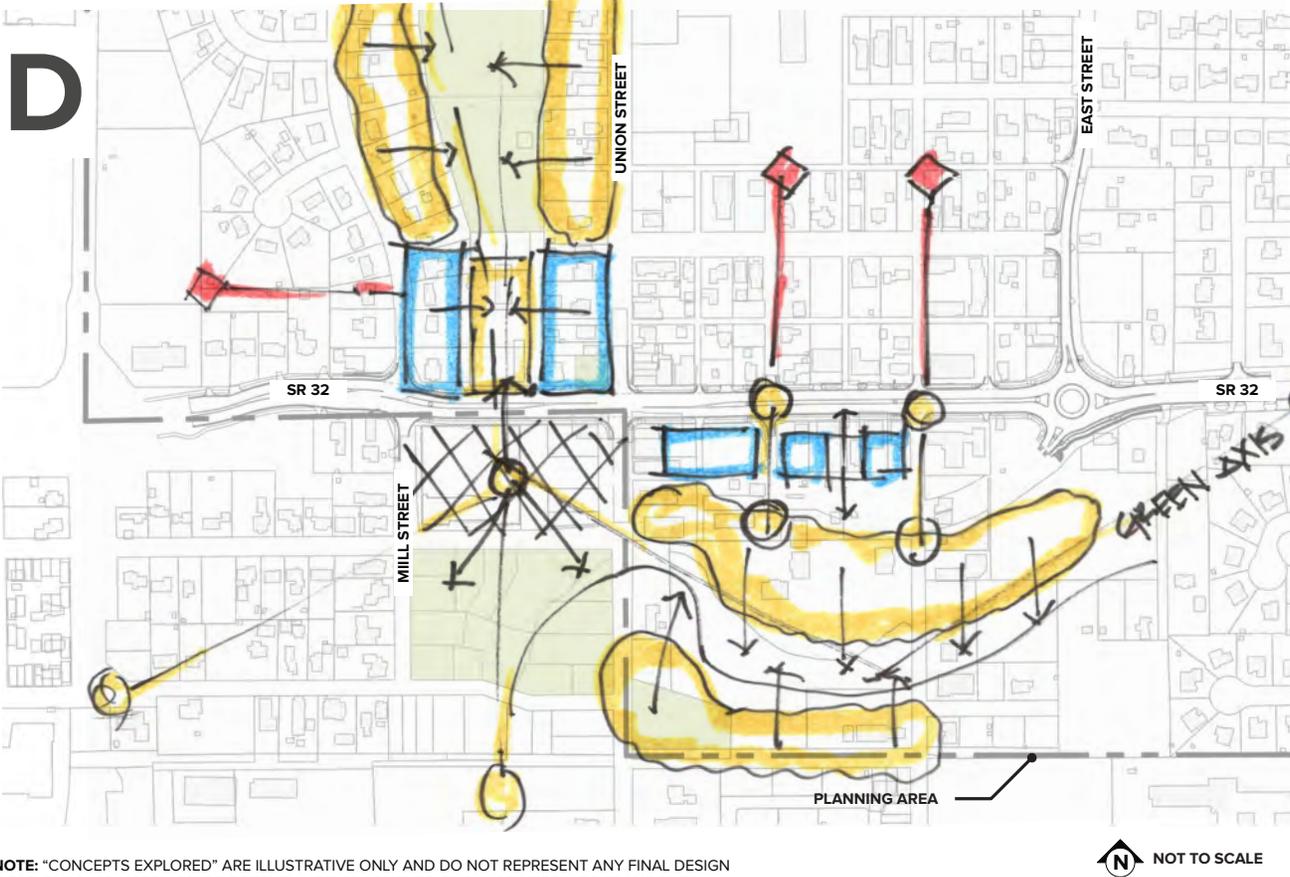
- Significant urban space at the creek ties the downtown spine to SR 32, creek becomes an amenity for housing east of Union.
- Historic zone along Union and civic plaza act as transitional buffer, marking the delineation between “doing” (work, shop, eat) and “living” spaces.
- Rowhouse developments vary the streetscape, keep things lower in height and massing, and provide a variety of pedestrian connections - *similar to older style cities (Boston)*
- Smaller retail activations along SR 32 create a movement pattern east to west with reasons to move between areas of downtown.
- Utilizing Jersey Street as a residential feeder for smaller (3 story) row house development could be an asset to keep “residential” feel of the eastern side of downtown - does that create a new delineation of downtown? Commercial happens west of union, living happens east... (typology axis)

GAPS EXPOSED

- Is residential as townhouses/rowhouses the correct approach to infill development south of SR 32 around Jersey / Cool Creek. Are we developing correctly at the right scale?

NOTE: “CONCEPTS EXPLORED” ARE ILLUSTRATIVE ONLY AND DO NOT REPRESENT ANY FINAL DESIGN IDEAS, BUILDING MASSINGS, OR POTENTIAL DEVELOPMENTS; THEY ARE MERELY TO FRAME THE OPPORTUNITIES AND GAPS WITHIN THE DOWNTOWN





**NOTE:** "CONCEPTS EXPLORED" ARE ILLUSTRATIVE ONLY AND DO NOT REPRESENT ANY FINAL DESIGN IDEAS, BUILDING MASSINGS, OR POTENTIAL DEVELOPMENTS; THEY ARE MERELY TO FRAME THE OPPORTUNITIES AND GAPS WITHIN THE DOWNTOWN

**D | "FOCUS ON GREEN"**

**CONCEPTS EXPLORED**

- City center plaza and municipal spaces located at Carnegie Library site and developed around a central extension/connection of Asa Bales Park
- Individual 3-5 story development parcels along north side of SR 32 develop over time (similar to size and scale of union square building) - *not shown, carried over from B*
- Green facing development occurs at either side of Asa Bales and Cool Creek; can include house, retail, planned open space, natural open space, etc

**OPPORTUNITIES REVEALED**

- By bringing the focus on the green (and blue) spaces, the downtown enhances and respects the assets of the existing natural topography, interest, and amenity (more green space than comparable north metro cities)
- Opportunities to enhance those assets with new development around green and waterways, similar to how the floodway was enhanced at Grand Junction Park.

**GAPS EXPOSED**

- What happens when there is no green resource to focus on, how do those connections build between green-focused development and those farther away?
- Could be perceived to "turn a back" on the existing fabric of downtown and beyond.





